



Federal Railroad Administration

May 16, 2025

Meg Kelly Coastal Management Program Director Illinois Dept. of Natural Resources 160 N LaSalle Street, Suite S-703 Chicago, IL 60601 Via email: Meg.Kelly@illinois.gov

Re: Amtrak Chicago River Wall Construction Phase 4

Coastal Zone Management Act (CZMA) Negative Determination

Dear Ms. Kelly:

This submission provides the Illinois Coastal Zone Management Program (CZMP) with the Federal Railroad Administration's (FRA) Negative Determination pursuant to the Coastal Zone Management Act (CZMA) of 1972, as amended (16 U.S.C. 1451 *et seq.*), and 15 Code of Federal Regulations (CFR) Part 930, Subpart C, for the funding of the Amtrak Chicago River Wall Construction Phase 4. FRA determined the proposed action will have no reasonably foreseeable effects on Illinois coastal uses or resources.

Description of Proposed Action

FRA is providing financial assistance to Amtrak to repair and replace river walls and additional support structures adjacent to the Amtrak Chicago Yard and other Amtrak assets along the South Branch Chicago River (Project). River walls line the South Branch Chicago River to help limiterosion and prevent flooding. Amtrak, as the owner of riverfront property, is responsible for maintaining the river wall adjacent to its property.

The purpose of the Project is to improve safety and serviceability of Amtrak assets along the river. The Project is needed due to the deterioration of the existing river walls and other support structures, such as bridge fenders.

The work is anticipated to occur during the 2025 construction season, and consists of the three packages described below, which are delineated based on the location of the work.

Package 1

Package 1 will consist of the replacement of approximately 700 feet of timber wall with new steel sheet pile wall along the west bank of the South Branch Chicago River between St. Charles Air Line Bridge and W. 18th Street Bridge. The replacement will occur in two sections: 540 feet of wall between the St. Charles Airline Bridge and the Continental Paper Grading Company building (1623 S Lumber St, Chicago, IL 60616) and 155 feet of wall just south of the Continental Paper Grading Company building. Approximately 150 feet of river wall in front of the Continental Paper Grading Company building and 280 feet of river wall north of the W. 18th Street Bridge were previously replaced with steel sheet pile. These wall sections appear to be in good condition and are not proposed to be replaced.

The new sheet pile wall will be constructed approximately two feet farther into the river, in front of the existing timber bulkhead wall. Portions of the deteriorated exposed timber bulkhead and outer piles will be removed to facilitate construction and anchoring of the new river wall system. The existing timber bulkhead wall will not be visible at the completion of work and the existing deadman system – a buried structure that forms an anchor – will remain in place.

The new walls will be attached to either a new deadman system located approximately 30 feet behind the new wall or pressure grouted ground anchors – threaded rods embedded in the existing ground for a length of approximately 50 feet. The deadman system would consist of steel sheet piling, walers, and tie rods and would require excavation to a depth of six to eight feet, depending on the existing slope of ground, to facilitate insertion of the new sheet piling and associated tie rods. The deadman system is not feasible at the locations of wall in front of existing Amtrak structures since it requires excavation. The pressure grouted ground anchors would not require additional excavation behind the new retaining wall to facilitate installation. Sheet piles along the water will be driven below the dredge surface at the river bottom. Once the new wall is constructed, slopes will be graded and revegetated; any disturbed parking areas will be repaved; and associated fencing and/or guardrail installed. No work is proposed under the St. Charles Airline Bridge or the W. 18th Street Bridge.

Amtrak also proposes to construct a small boat launch at the north end of the Package 1 study area. Amtrak staff currently use a wood staircase at this location for river access to complete minor maintenance activities on the South Branch Chicago River. Package 1 would replace the existing wood stairs with concrete stairs and a landing inset to the river wall (i.e., the proposed boat launch would not stick out beyond the plane of the river wall).

Package 2

Package 2 consists of in-kind concrete repairs to approximately 110 feet of the existing masonry river wall along the west bank of the South Branch Chicago River north of the W. Van Buren Street Bridge. These repairs will include cleaning and patching deteriorated sections of the existing river wall above and below the water line and will not involve any repairs on the W. Van Buren Street Bridge elements. Further, to ensure the structural integrity of Amtrak's access road that traverses the west bank of the South Branch Chicago River, Amtrak is proposing in-

kind concrete repairs to approximately 60 feet of a remnant pier in the river. Lastly, Amtrak is proposing approximately 90 feet of formed concrete repairs to a guardrail foundation.

Package 3

Package 3 includes the replacement of approximately 110 feet of river wall along the west bank of the South Branch Chicago River immediately north of the South Branch Bridge (Pennsylvania Railroad Bridge 466) as well as the repair of the existing fender system of the South Branch Bridge. The South Branch Bridge, built in 1914, carries freight railroads, Metra, and Amtrak across the South Branch Chicago River. The bridge is nearly 300 feet long and consists of five spans: a lift span, two tower spans, and an approach span on each side. The tower spans are protected by the timber fender system, which has localized deterioration requiring repairs.

The new river wall will be a cantilevered soldier pile wall, which will be constructed approximately two feet in front of the existing timber bulkhead wall. In addition, Package 3 will repair or replace an existing stacked timber wall on the east bank north of the bridge within railroad right of way. At the west bank, portions of the deteriorated exposed timber bulkhead and outer piles will be removed to facilitate construction and anchoring of the new river wall system. The existing timber bulkhead wall will not be visible at the completion of work.

The new soldier pile wall system would consist of steel H-piles driven along the river edge extending beneath the river bottom combined with precast concrete lagging placed between the piles down to the river bottom. Once the new wall is constructed, slopes will be graded and revegetated; any disturbed parking areas will be repaved; and associated fencing and/or guardrail installed.

The work on the timber fender system consists of in-kind repair or replacement of individual timber elements. The repairs will be near the water line elevation and above. Certain individual pile elements will be spliced with new timber, but work on the piles is not anticipated to extend to the river bottom.

A Negative Determination is required because the Project requires a Section 404/408 Permit and a 3704 Permit. Amtrak will obtain the Section 404/408 Permit from the US Army Corps of Engineers (USACE) and the 3704 Permit from the Illinois Department of Natural Resources (IDNR) prior to commencement of construction. Amtrak will comply with all permit requirements.

Effects of the Proposed Action

FRA anticipates no changes to existing or future land use; no impacts to federally listed threatened or endangered species under Section 7 of the Endangered Species Act; no impacts to parks or recreational facilities; and no changes in access to coastal resources. In addition, the Project will not acquire any right-of-way or permanent easements. For Package 3 construction, Amtrak will require access to the property at 329 W. 18th Street, north of the Pennsylvania Railroad Bridge 466, to tie the proposed improvements of Package 3 into the existing river wall.

Amtrak will coordinate an agreement with the adjacent private property owner for access to the private property for construction of the cantilevered soldier pile wall.

Amtrak will coordinate with the City of Chicago to avoid impacts to river activities, including the Chinese Dragon Boat Race and the Chicago Water Taxi service. Amtrak's construction contractor will establish and maintain appropriate navigational safety measures during construction operations from barges in the South Branch Chicago River.

Though impacts to the South Branch Chicago River are anticipated, these impacts are anticipated to be minimal. Amtrak's initial coordination with USACE suggests that the project may be permitted within the nationwide program. The permit type is up to the final discretion of USACE. Further, IDNR – Office of Water Resources indicated to Amtrak that no hydraulic analysis would be required so long as the replacement wall did not encroach more than three (3) feet into the river, which the Project will not.

Review of the Project under Section 106 of the National Historic Preservation Act is underway, and no adverse effects to historic properties are anticipated.

Based on the scope of work, the Project will have no reasonably foreseeable effects on the coastal use or resources of the Illinois coastal zone.

Pursuant to 15 CFR § 930.35(c), the Illinois CZMP has 60 days from the receipt of this letter to concur with or object to this negative determination, or to request an extension. Illinois' concurrence will be presumed if Illinois' response is not received by FRA on the 60th day from receipt of this letter.

If you have any questions or require additional information, please contact me at , or Ryan Snyder, Amtrak Lead NEPA Specialist, at

Thank you for working with FRA and Amtrak on this important rail project.

Sincerely,

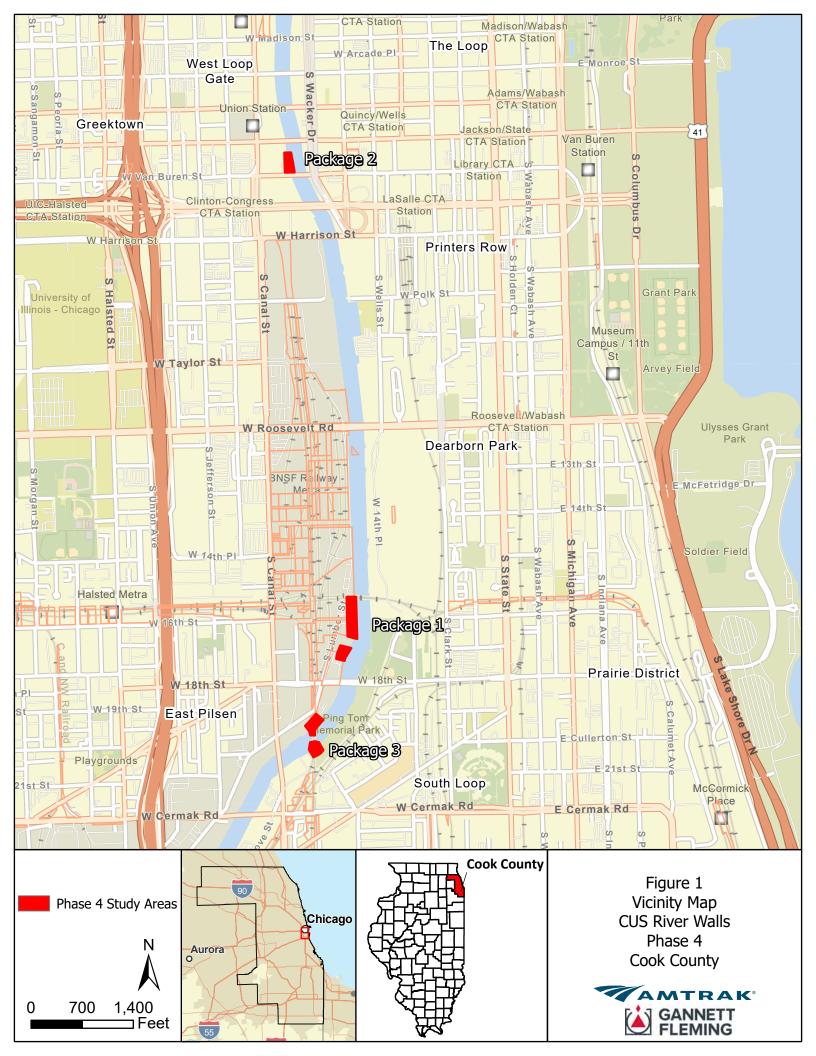
Anthony Ross
Environmental Protection Specialist
Environmental Review Division
Office of Environmental Program Management

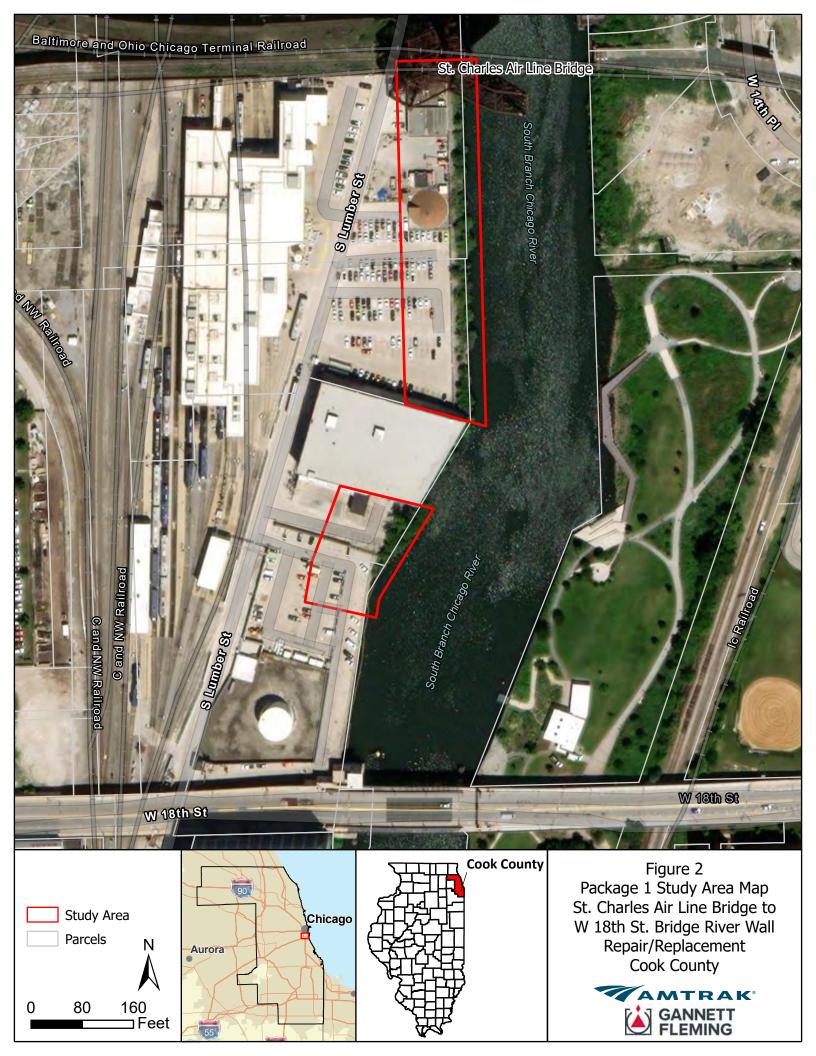
Enclosures:

Study Area Map 60% Plan Submittal

cc: Cody Eskew, CZMP Federal Consistency Contact Ryan Snyder, Amtrak

Concurrence:	
	Date:
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Meg Kelly	
Coastal Management Program Director	
Illinois Dept. of Natural Resources	
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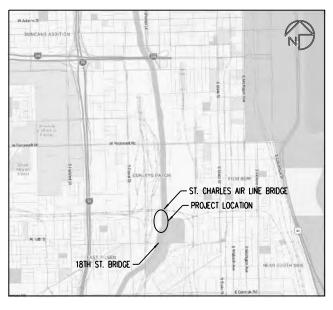






CHICAGO RIVER WALL CONSTRUCTION PHASE 4

CHICAGO, IL PACKAGE 1



DWG. NO.	SHT. NO.	DRAWING TITLE		
X-100	1	COVER SHEET		
G-101	2	GENERAL NOTES AND ESTIMATED QUANTITIES		
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PROJECT SCOPE SUMMARY

THE AMTRAK CHICAGO RIVER WALL IS LOCATED ALONG THE WEST BANK OF THE SOUTH BRANCH OF THE CHICAGO RIVER NEAR THE AMTRAK CHICAGO YARD IN CHICAGO, IL. A PREVIOUS SECTION OF RIVER WALL EXTENDING FROM THE ST. CHARLES AIR LINE BRIDGE TO THE POWER PLANT AT THE NORTH END OF THE CHICAGO YARD WAS REPLACED BETWEEN 2019–2021 WITH STEEL SHEET PILE WALL WITH A TIE-BACK ANCHOR SYSTEM. EXISTING TIMBER RIVERWALL SEGMENTS, SOUTH OF THE PREVIOUS WORK, EXTENDING FROM THE ST. CHARLES AIR LINE BRIDGE TO THE W. 18TH ST. BRIDGE ARE IN POOR CONDITION AND SHOW SIGNS OF DETERIORATION. 656-FT OF EXISTING TIMBER RETAINING WALL SECTIONS WILL REMAIN IN PLACE AND HAVE NEW STEEL SHEET PILE WALLS WITH TIE-BACK ANCHOR SYSTEMS CONSTRUCTED IN FRONT OF THE EXISTING WALL. AN ADDITIONAL 44-FT SECTION OF EXISTING TIMBER RETAINING WALL. WAN ADDITIONAL 44-FT SECTION OF EXISTING TIMBER RETAINING WALL WILL REMAIN IN PLACE AND HAVE A STSTEMS CONSTROCTED IN TROVIT OF THE EXISTING WALL AN AUTHORIZED ASSECTION OF EXISTING TIMBER RETAINING WALL WILL REMAIN IN PLACE AND HAVE A STEEL SOLDIER PILE RETAINING WALL WITH PRECAST CONCRETE LAGGING. THE EXISTING TIMBER BOAT DOCK AND STAIRS APPROXIMATELY 90-FT SOUTH OF THE ST. CHARLES AIR LINE BRIDGE WILL ALSO BE REPLACED WITH A NEW, WIDER CAST-IN-PLACE CONCRETE LANDING AND STAIRS.

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Office of Chief Engineer STRUCTURES

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COOK
CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1

COVER SHEET

Designed JGC Drawn ACB Checked JGC Date \$DATE

GENERAL NOTES:

 ALL REQUIREMENTS SHOWN ON THESE DRAWINGS SHALL BE ACCOMPLISHED AS SPECIFIED IN AMTRAK EP-3014 SPECIFICATIONS AND THE MOST CURRENT EDITION OF THE AMERICAN RAILWAY ENGINEERING AND MAINTENANCE-OF-WAY ASSOCIATION (AREMA) MANUAL FOR RAILWAY ENGINEERING.

CHAPTER 8 CONCRETE STRUCTURES AND FOUNDATIONS

CHAPTER 15 STEEL STRUCTURES

- ESTIMATED QUANTITIES INDICATED ARE APPROXIMATE BASED ON PLAN
 REQUIREMENTS AND DO NOT INCLUDE ANY ADDITIONAL REQUIREMENTS WHICH MAY
 BE DETERMINED BY THE ENGINEER IN FIELD.
- DIMENSIONS AND EXISTING CONDITIONS SHALL BE FIELD VERIFIED PRIOR TO THE FABRICATIONS OF ANY MATERIAL. IF DRAWINGS AND SPECIFICATIONS CONFLICT, THE DRAWINGS SHALL GOVERN IN FIELD.
- 4. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING STRUCTURES HAVE BEEN TAKEN FROM EXISTING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THESE DIMENSIONS AND SUBMIT ANY DEVIATIONS WITH SUPPORTING SURVEY NOTES AND DRAWINGS FOR REVIEW AND APPROVAL. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR CHANGES IN SCOPE OF WORK. CONTRACTOR WILL BE PAID FOR ACTUAL APPROVED QUANTITY IN THE FIELD. VERIFICATION SHALL BE DONE BEFORE THE COMMENCEMENT OF CONSTRUCTION AND PREPARATION OF THE SHOP DRAWINGS.
- 5. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

RIVER WALL NOTES:

- 1. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH AMTRAK.
- FASTENERS SHALL BE ASTM A440 HIGH STRENGTH BOLTS HOT-DIP GALVANIZED, TYPE I. ALL BOLTS % DIAMETER WITH 15%" DIAMETER OPEN HOLES, UNLESS OTHERWISE NOTED.
- SPACERS SHALL BE %" DIAMETER ASTM F3125, GRADE A325 HIGH STRENGTH BOLTS AND 34" DIAMETER PIPE SPACERS HOT-DIP GALVANIZED, TYPE I.
- 4. ALL STRUCTURAL STEEL AND STRUCTURAL PLATES UNLESS NOTED OTHERWISE SHALL BE ASTM A572 GRADE 50. ALL NEW STEEL SHALL BE HOT-DIP GALVANIZED PER ASTM A123. CONTRACTOR SHALL COLD GALVANIZE WELDS AND DAMAGES TO GALVANIZED MEMBERS.
- 5. WELDS TO BE E70XX PER AWS D1.
- STRUCTURAL CONCRETE MUST HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI. CONCRETE SHALL BE CLASS DS PER IDOT SPECIFICATIONS.
- 7. ALL HOT ROLLED STEEL SHEET PILING SHALL BE ASTM A690 GR. 50 KSI STEEL.
- 8. THE CONTRACTOR IS RESPONSIBLE FOR THE FULL COST TO REPAIR AND RESTORE UTILITIES DAMAGED BY HIS OPERATION.
- THE CONTRACTOR SHALL MAKE HIS OWN INVESTIGATION TO DETERMINE THE EXISTENCE, NATURE AND EXACT LOCATION OF ALL UTILITY LINES AND APPURTENANCES WITHIN THE LIMITS OF THE WORK, ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 10. CONTRACTOR SHALL PROTECT AND/OR RELOCATE ALL UTILITIES, WHICH WOULD BE AFFECTED BY THE CONSTRUCTION. PIPES/DUCTS, FITTINGS, AND INSERTS ARE TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO THE MANUFACTURER'S RECOMMENDATION AND OWNERS REQUIREMENTS. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.

RIVER WALL NOTES (CONT.):

- 11. THE CONTRACTOR SHALL EXERCISE CAUTION DURING ALL CONSTRUCTIONS
 OPERATIONS TO PREVENT ANY DAMAGE TO ADJACENT STRUCTURES, UTILITIES, AND
 BUILDING COMPONENTS NOT WITHIN THE SCOPE OF THIS WORK. OTHER MEMBERS,
 STRUCTURES, UTILITIES, AND STRUCTURAL COMPONENTS NOT WITHIN THE SCOPE OF
 THESE IMPROVEMENTS THAT ARE DAMAGED DURING THE CONSTRUCTION OPERATIONS
 SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR AND TO
 THE SATISFACTION OF THE ENGINEER.
- 12. THE CONTRACTOR SHALL ESTABLISH SURVEY POINTS, BENCH MARKS AND ALL NECESSARY CONTROL POINTS TO CONSTRUCT THE PROPOSED PROJECT AND ITS INDIVIDUAL ELEMENTS. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 13. THE U.S. ARMY CORPS OF ENGINEERS, THE ILLINOIS DEPARTMENT OF NATURAL RESOURCES, THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND HARBOR PERMITS FOR THE PROPOSED RIVER WALL WORK WILL BE SECURED BY AMTRAK AND THEIR REPRESENTATIVES. THE SECURING AND OBTAINING OF ALL OTHER NECESSARY CONSTRUCTION PERMITS FOR THE SPECIFIED WORK SHALL BE SOLE RESPONSIBILITY OF THE CONTRACTOR, INCLUDING THE CDOT OFFICE OF UNDERGROUND COORDINATION PERMIT
- 14. ALL WORK SHALL BE PERFORMED SO AS NOT TO INTERFERE WITH THE FREE FLOW OF NAVIGATION AND SUCH THAT THE NAVIGABLE DEPTHS AND CLEARANCES WITHIN THE CANAL ARE NOT IMPAIRED. ANY WORK EQUIPMENT/BARGES LOCATED WITHIN THE WATERWAY SHALL BE MOVABLE AT ANY TIME, AND THE CONTRACTOR SHALL NOTIFY AND RECEIVE APPROVAL FROM THE U.S. COAST GUARD AS TO MOORING THE BARGES AT THE WORK SITE. ANY WORK BARGE OR OTHER EQUIPMENT IN THE NAVIGABLE CHANNEL BE IDENTIFIED WITH THE NECESSARY LIGHTS AND SIGNALS REQUIRED BY "INLAND NAVIGATIONAL RULES". THE LOCATION AND TIMES OF PLACEMENT OF EQUIPMENT IN THE CANAL SHALL BE REPORTED TO AND APPROVED BY ALL OF THE NECESSARY AUTHORITIES.
- 15. ALL DEBRIS RESULTING FROM THE REMOVAL OF THE EXISTING STRUCTURES OR CREATED WHILE PERFORMING THE SPECIFIED WORK SHALL BE REMOVED FROM THE PROJECT SITE. NO MATERIAL SHALL BE DISCHARGED INTO THE WATER/RIVER. THE CONTRACTOR SHALL IMPLEMENT PROTECTIVE MEASURES SO AS TO PREVENT ANY DEBRIS FROM FALLING INTO THE CHICAGO RIVER. IF ANY DEBRIS FALLS INTO THE WATERWAY, THE CONTRACTOR SHALL REMOVE IT FROM THE RIVER AT HIS/HER EXPENSE AND TO THE ENGINEER'S SATISFACTION, ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 16. DEBRIS AND HAZARDOUS SUBSTANCES SHALL BE DISPOSED OF IN ACCORDANCE WITH THE REGULATIONS OF ALL GOVERNING AGENCIES. THE REMOVAL OF HAZARDOUS SUBSTANCES AND OBTAINING THE PERMITS FOR SUCH DISPOSAL SHALL BE SOLE REPSONSIBILITY OF THE CONTRACTOR. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 17. ALL ELEVATIONS REFER TO CHICAGO CITY DATUM.
- 18. AFTER CONSTRUCTION HAS BEEN COMPLETED ALL EXCESS MATERIALS AND TRASH SHALL BE REMOVED AND DISPOSED OF AT AN OFF-SITE WASTE FACILITY. THE SITE SHALL BE BROOM CLEANED AND WASHED DOWN LEAVING THE PREMISES IN A CLEAN CONDITION AND SAFE CONDITION. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- PROPOSED SHEET PILE WALL SHALL BE CONSTRUCTED IN PHASES. THE NUMBER AND LIMITS OF PHASES WILL BE DETERMINED BY AMTRAK PRIOR TO START OF CONSTRUCTION.
- 20. CONTRACTOR SHALL REFER TO AMTRAKCONTRACTOR.COM FOR REQUIREMENTS AND DETAILS REGARDING EMPLOYEE SCREENING REQUIREMENTS.

GROUND ANCHOR NOTES:

- 1. DESIGN CAPACTIY OF TIEBACKS (WORKING LOAD) = 189 KIPS.
- TIEBACKS TO BE INSTALLED AT EVERY OTHER SHEETING PAIR (7'-6½" MAX).
 TIEBACK MAY BE OFFSET TO MISS SHEETING INTERLOCK PROVIDED TIEBACK BRACKET IS IN FULL CONTACT WITH SHEETING PAN.
- TIEBACKS SHALL BE PROOF TESTED IN ACCORDANCE WITH PTI RECOMMENDATIONS/SPECIFICATIONS.

GROUND ANCHOR NOTES (CONTINUED):

- 4. TIEBACK DETAILS PROVIDED ARE GENERAL REQUIREMENTS BASED ON PROJECT PARAMETERS. SPECIFIC TIEBACK DETAILS AND REQUIRED ACCESSORIES FOR INSTALLATION SHALL REMAIN THE RESPONSIBILITY OF THE TIEBACK INSTALLER.
- 5. [1'-¾"] DIA. (GRADE 150) DYWIDAG RODS ARE SHOWN IN ALL SECTIONS/DETAILS.

 (4)-.6" DIA (GRADE 270) PRESTRESSING STRANDS OR AN APPROVED EQUIVALENT

 ROD MAY BE USED IN LIEU OF DYWIDAG RODS SHOWN. SPECIFIC DETAILS ARE THE

 RESPONSIBILITY OF THE TIEBACK INSTALLER.
- 6. ALL 1 3/8" DIA. PRESTRESSING STEEL THREADBAR SHALL BE ASTM A722, GR. 150.
- THREADBAR SHALL HAVEDYWIDAG DOUBLE CORROSION PROTECTION (DCP) OR EQUIVALENT.
- PLATES, NUTS, WASHERS, BOLTS, PIPE SEPARATORS, BRACKETS, PLATE STIFFENERS, AND WALERS SHALL BE GALVANIZED.
- 9. GROUND ANCHORS SHALL BE PRESSURE GROUTED.

SITE SECURITY & TRAFFIC MANAGEMENT:

- CONTRACTOR IS RESPONSIBLE FOR SITE SECURITY DURING CONSTRUCTION, ESPECIALLY DURING NON-WORK HOURS.
- 2. CONTRACTOR SHALL REPLACE ALL SITE SECURITY COMPONENTS TO MATCH THE EXISTING LOCATIONS, SUCH AS BUT NOT LIMITED TO, CHAINLINK FENCES, GATES, GUARDRAILS, POLES, AND ALL UTILITIES AND ELECTRICAL EQUIPMENT THAT IS ASSOCIATED WITH THE SECURITY COMPONENTS. COMPONENTS NOT WITHIN THE SCOPE OF THESE IMPROVEMENTS THAT ARE DAMAGED DURING THE CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER.

SOIL MANAGEMENT:

I. CONTRACTOR SHALL IMMEDIATELY NOTIFY AMTRAK UPON OBSERVATION OF ENVIRONMENTAL CONTAMINATION (E.G. PETROLEUM LIKE ODORS, STAINING OR FREE PRODUCT). CONTAMINATED SOILS SHALL BE STOCKPILED SEPARATELY FROM OTHER SOILS. CONTAMINATE SOILS SHALL BE STOCKPILED ON EITHER 6-MIL POLY OR AN IMPERMEABLE SURFACE SUCH AS ASPHALT OR CONCRETE. THE CONTAMINATED SOIL STOCKPILE SHALL BE SECURELY COVERED WITH 6-MIL POLYETHYLENE SHEETING AT THE END OF EACH WORK DAY. SHEETING SHALL BE SECURELY WEIGHTED DOWN. SOILS THAT DO NOT EXHIBIT CONTAMINATION ARE SUITABLE FOR REUSE BY THE CONTRACTOR AS BACKFILL. ALL [EXCESS] SOIL WILL BE TESTED AND DISPOSED BY AMTRAK. REFER TO SECTION [026100] IN THE CONTRACTOR SPECIFICATIONS FOR "REMOVAL AND DISPOSAL OF CONTAMINATED SOILS".

CONSTRUCTION SEQUENCE (GROUND ANCHORS):

- 1. LOCATE ALL UTILITIES AND FOUNDATIONS.
- LOCALLY REMOVE PORTIONS OF EXISTING PILES AND CAP AS REQUIRED TO PROPERLY INSTALL NEW SHEET PILING.
- 3. DRIVE NEW SHEET PILING [2'-0"] IN FRONT OF EXISTING BULKHEAD WALL.
- PLACE AND COMPACT GRANULAR FILL TO ELEVATION [+0.0'] C.C.D. BETWEEN SHEETING AND EXISTING TIMBER WALL.
- 5. EXCAVATE TO EL. [+1.0] C.C.D.
- 6. INSTALL WALERS AND TIEBACKS AT EL. [+2.5'] C.C.D.
- PLACE AND COMPACT GRANULAR FILL TO ELEVATION SHOWN ON CONTRACT PLANS.
- 8. PLACE SUBGRADE.

TOTAL BILL OF MATERIAL

ITEM		
ITEM	UNIT	QUANTITY
CIVIL SITE WORK	L. SUM	
CONCRETE STRUCTURES	C.Y.	
PERMANENT SHEET PILING	S.F.	
FURNISHING SOLDIER PILES (W SECTION)	L.F.	
PRECAST CONCRETE LAGGING	S.F.	
PERMANENT GROUND ANCHORS	EA.	
CONCRETE ENCASEMENT	C.Y.	
PERMANENT STEEL CASING	L.F.	
CONTROLLED LOW-STRENGTH MATERIAL	C.Y.	
FURNISHING AND ERECTING STRUCTURAL STEEL	LBS	

* THE QUANTITIES PROVIDED ABOVE ARE FOR BIDDING PURPOSES ONLY. THESE QUANTITIES MUST BE ADJUSTED IN THE FIELD BY THE ENGINEER AS CONDITIONS WARRANT.

No. Revisions Date By
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Office of Chief Engineer
STRUCTURES

National Rallroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

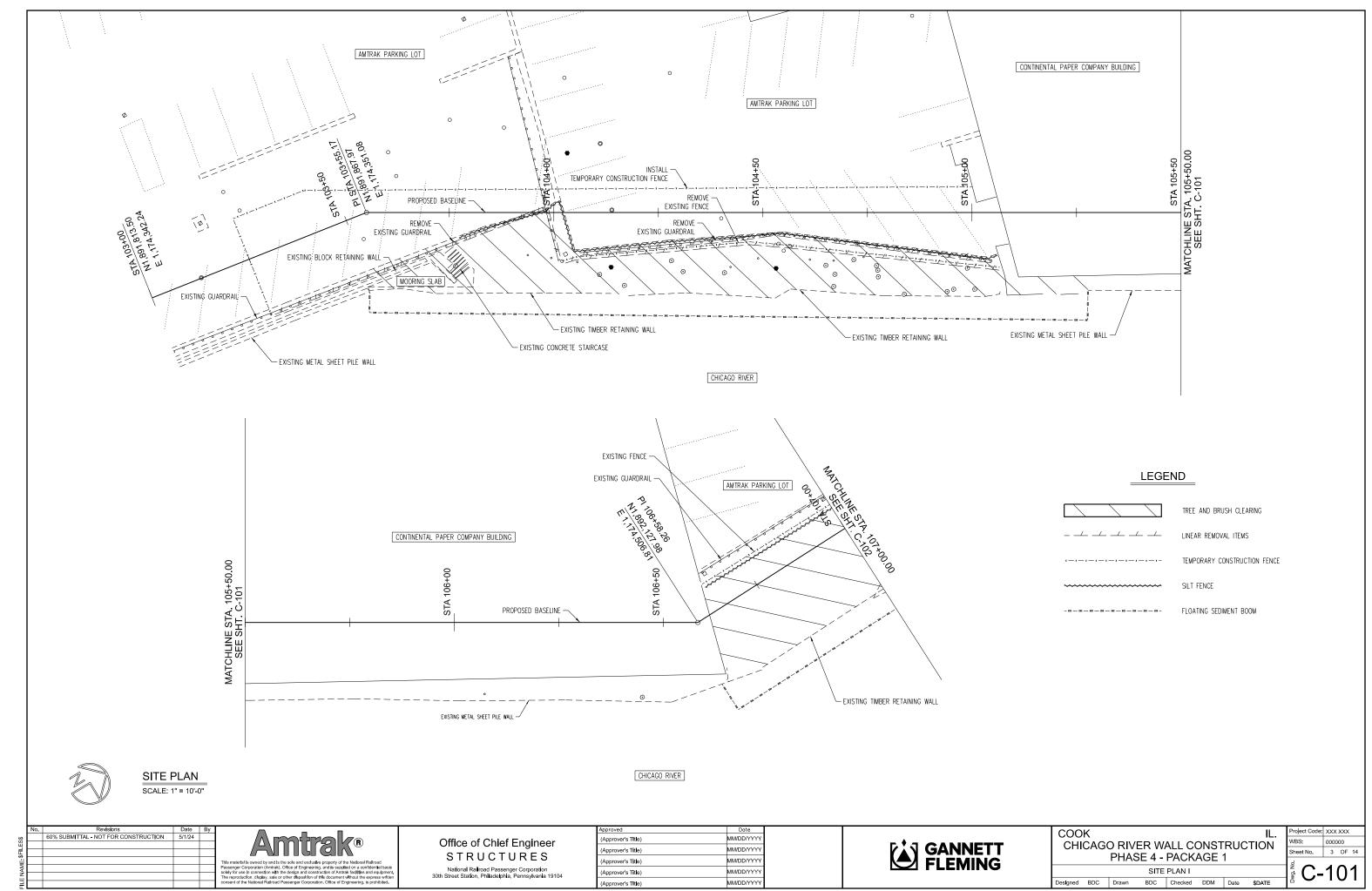
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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1

Sheet No. 2 OF 14

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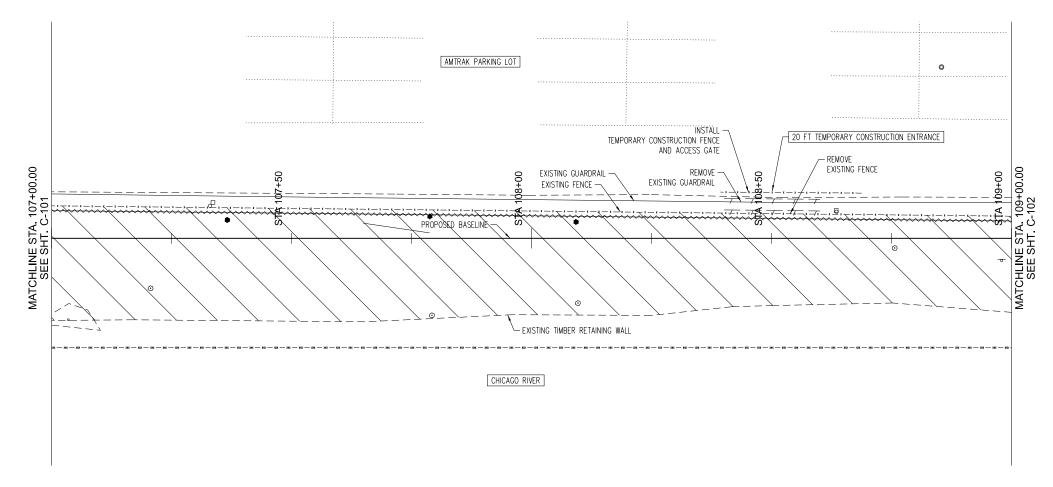
TREE AND BRUSH CLEARING

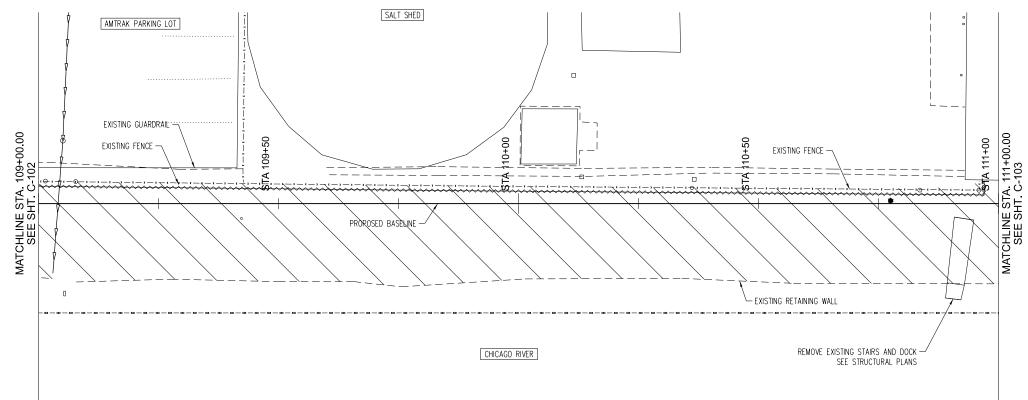
LINEAR REMOVAL ITEMS

TEMPORARY CONSTRUCTION FENCE

SILT FENCE

FLOATING SEDIMENT BOOM







SITE PLAN

SCALE: 1" = 10'-0"

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Amtrak®

Office of Chief Engineer STRUCTURES

National Railroad Passenger Corporation 30th Street Station, Philadelphia, Pennsylvania 19104

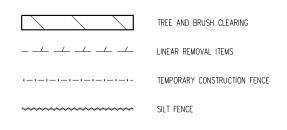
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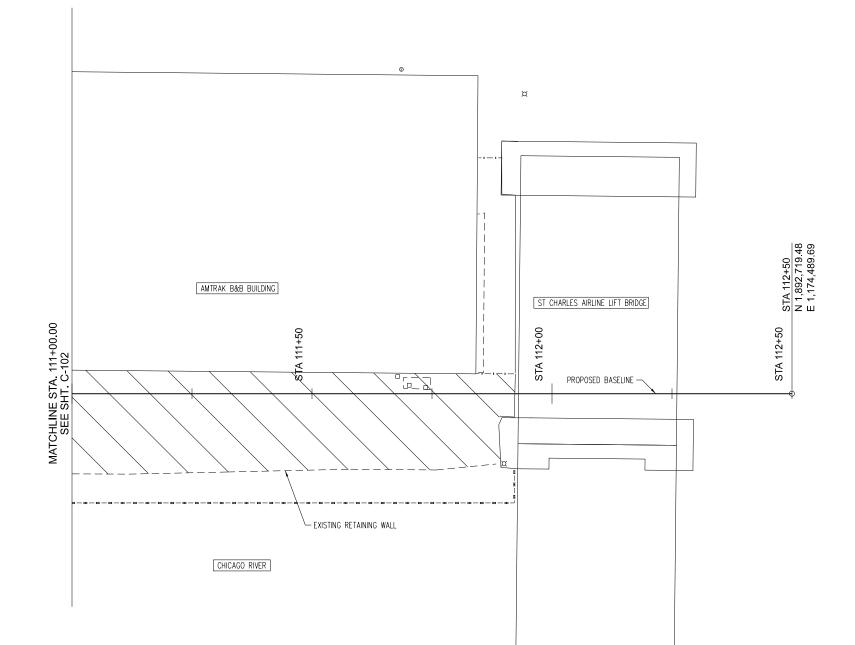


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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1
SITE PLAN II

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Sheet No. 4 OF 14







SITE PLAN

SCALE: 1" = 10'-0"

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National Railroad Passenger Corporation 30th Street Station, Philadelphia, Pennsylvania 19104

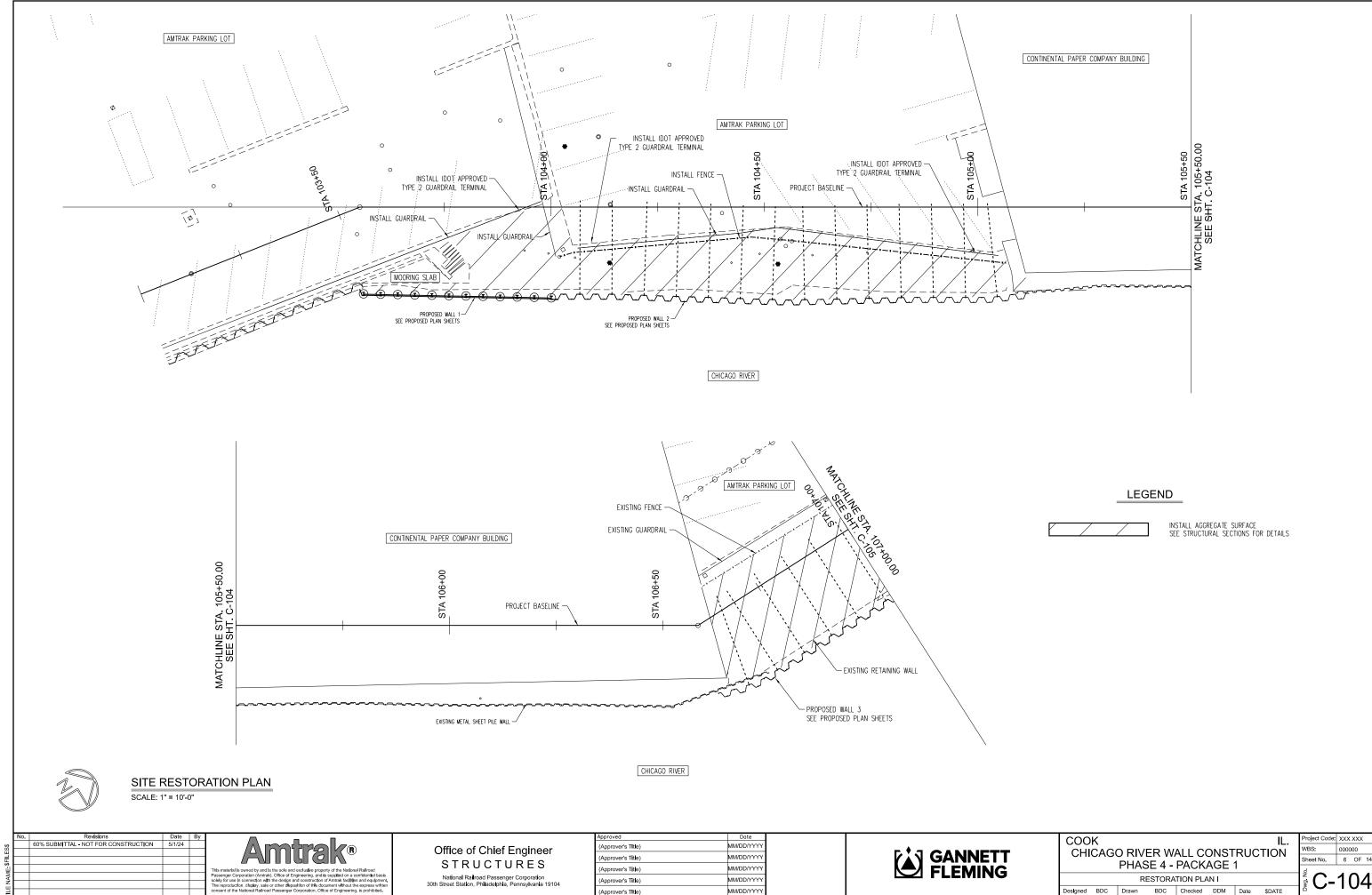
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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1
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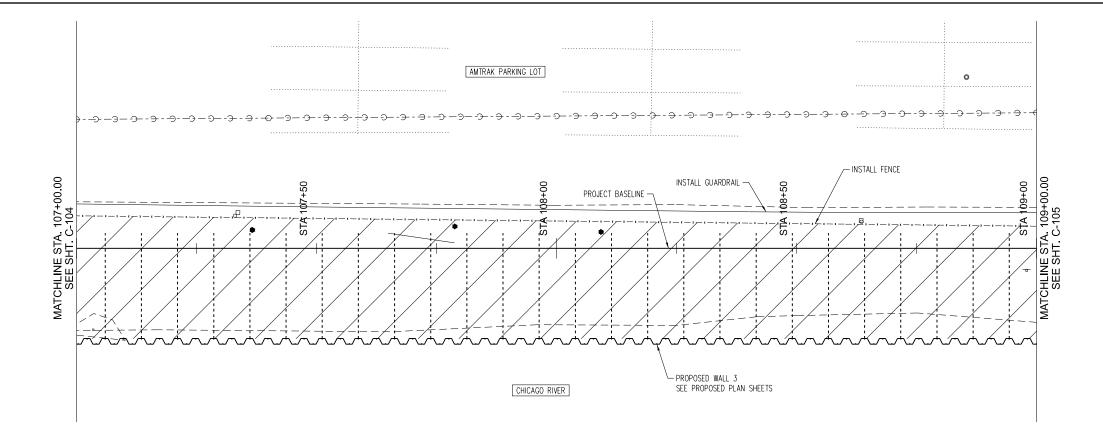
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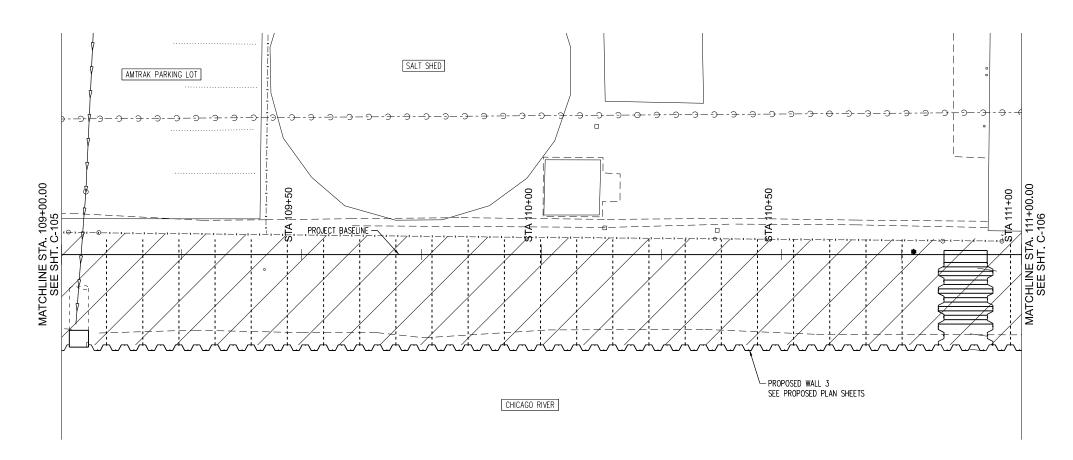
Sheet No. 5 OF 14



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INSTALL AGGREGATE SURFACE SEE STRUCTURAL SECTIONS FOR DETAILS







SITE RESTORATION PLAN
SCALE: 1" = 10'-0"

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Amtrak®

Office of Chief Engineer STRUCTURES

National Railroad Passenger Corporation 30th Street Station, Philadelphia, Pennsylvania 19104

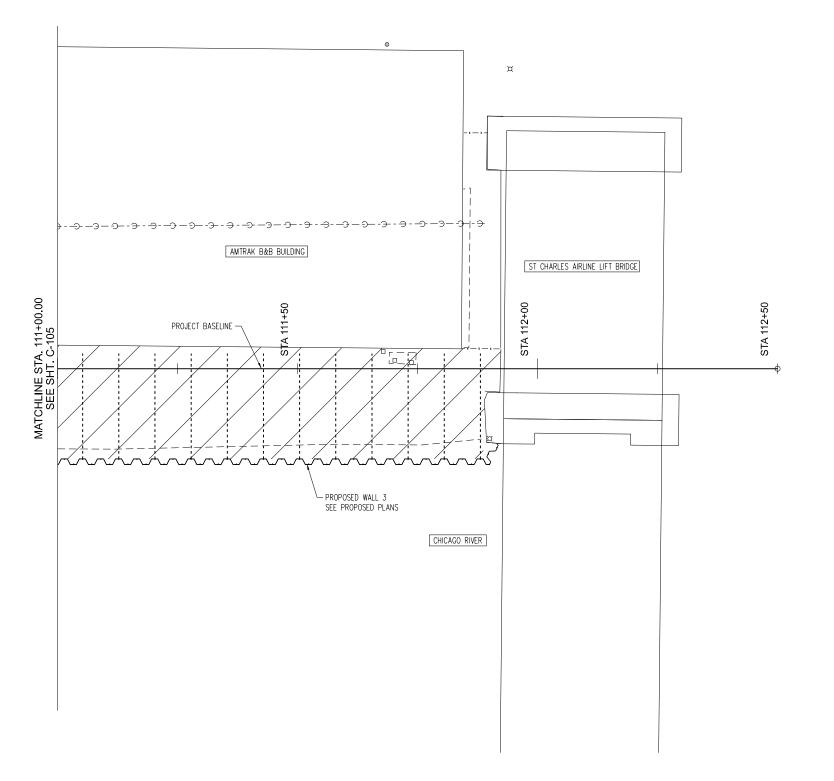
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COOK IL.
CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1
RESTORATION PLAN II

Designed BDC Drawn BDC Checked DDM Date \$DATE

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	Sheet No.	7 OF 14
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INSTALL AGGREGATE SURFACE SEE STRUCTURAL SECTIONS FOR DETAILS



SITE RESTORATION PLAN
SCALE: 1" = 10'-0"

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30th Street Station, Philadelphia, Pennsylvania 19104

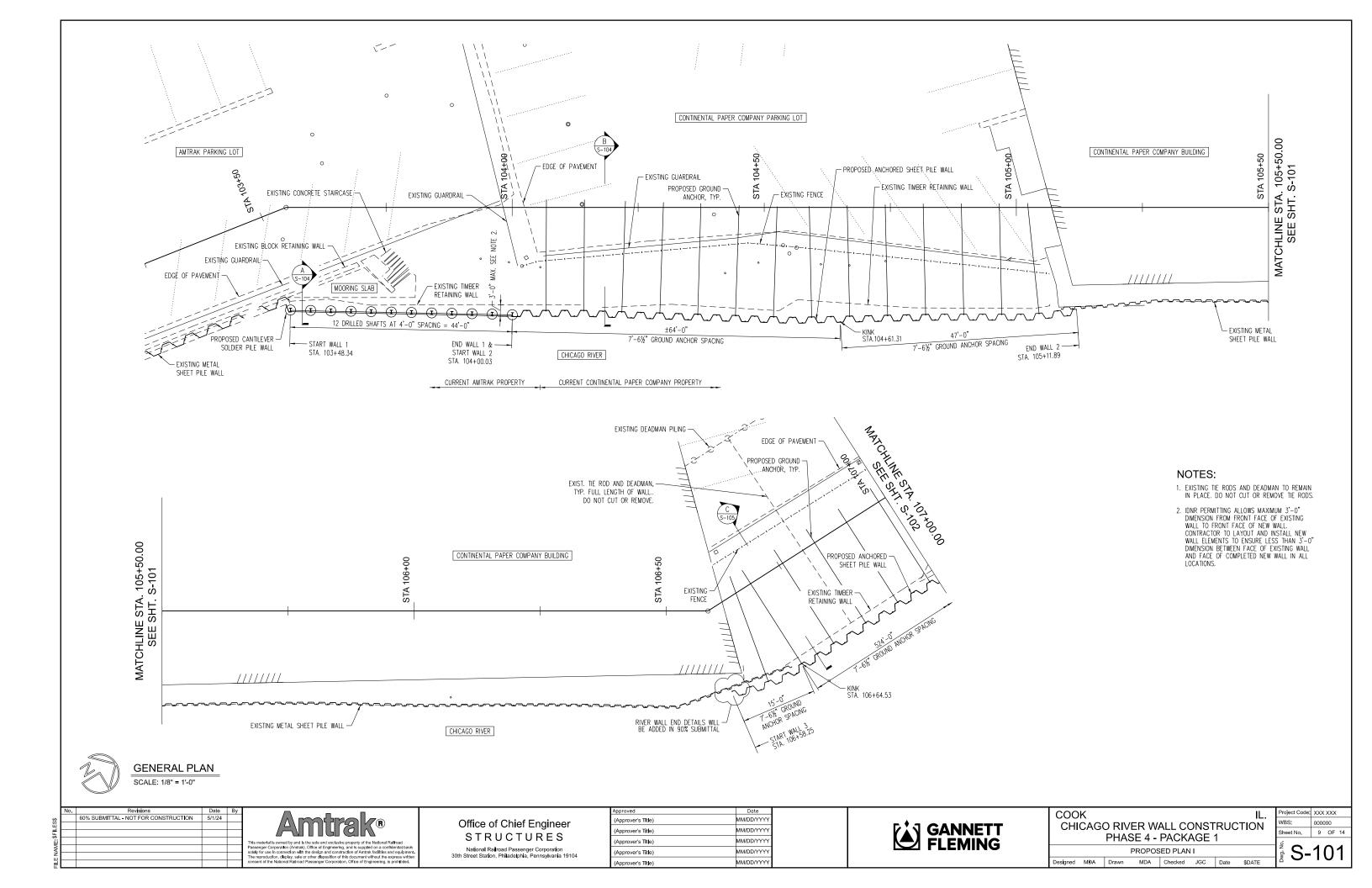
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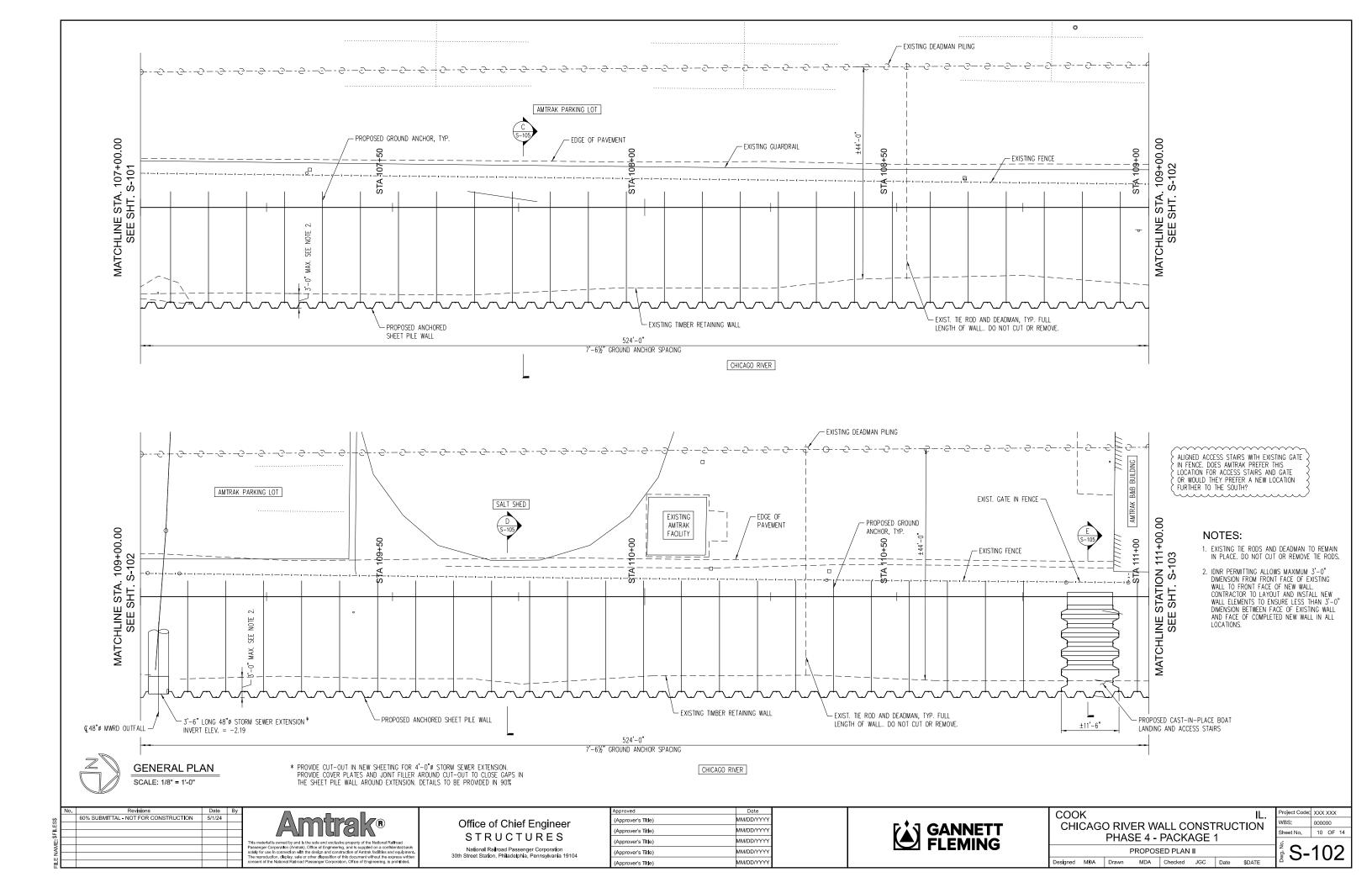


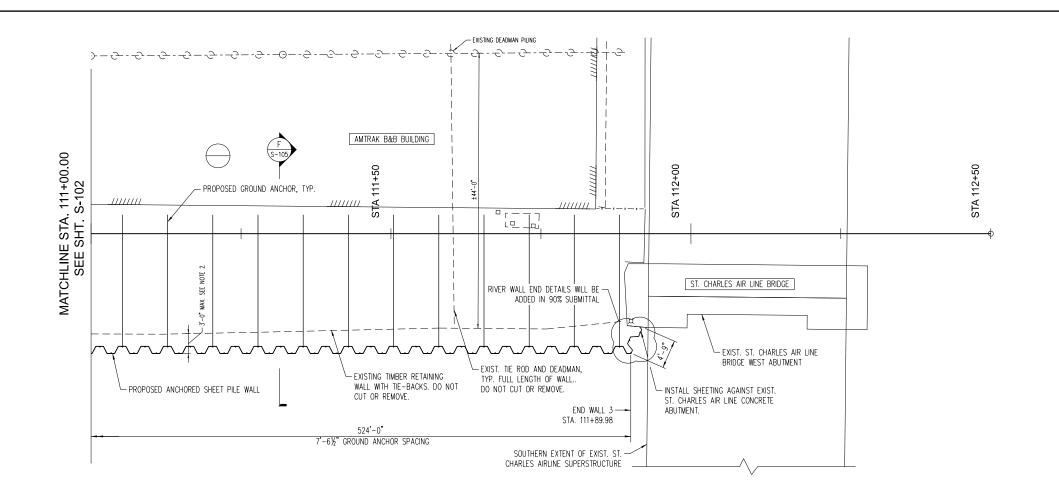
COOK IL.
CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1
RESTORATION PLAN III

Designed BDC Drawn BDC Checked DDM Date \$DATE

Project Code: XXX XXX WBS: 000000 Sheet No. 8 OF 14







NOTES:

- 1. EXISTING TIE RODS AND DEADMAN TO REMAIN IN PLACE. DO NOT CUT OR REMOVE TIE RODS.
- 2. IDNR PERMITTING ALLOWS MAXIMUM 3'-0"
 DIMENSION FROM FRONT FACE OF EXISTING
 WALL TO FRONT FACE OF NEW WALL.
 CONTRACTOR TO LAYOUT AND INSTALL NEW
 WALL ELEMENTS TO ENSURE LESS THAN 3'-0"
 DIMENSION BETWEEN FACE OF EXISTING WALL
 AND FACE OF COMPLETED NEW WALL IN ALL
 LOCATIONS.



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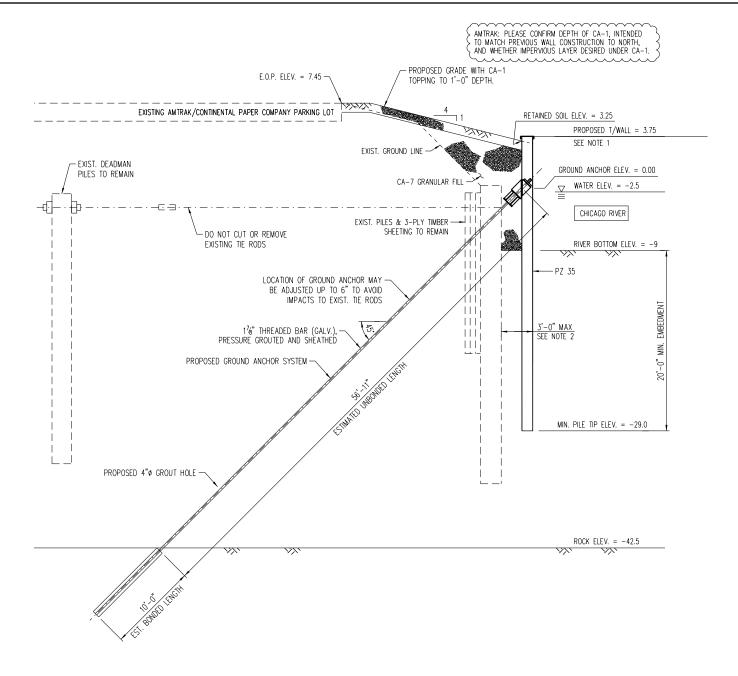
Office of Chief Engineer STRUCTURES National Railroad Passenger Corporation 30th Street Station, Philadelphia, Pennsylvania 19104

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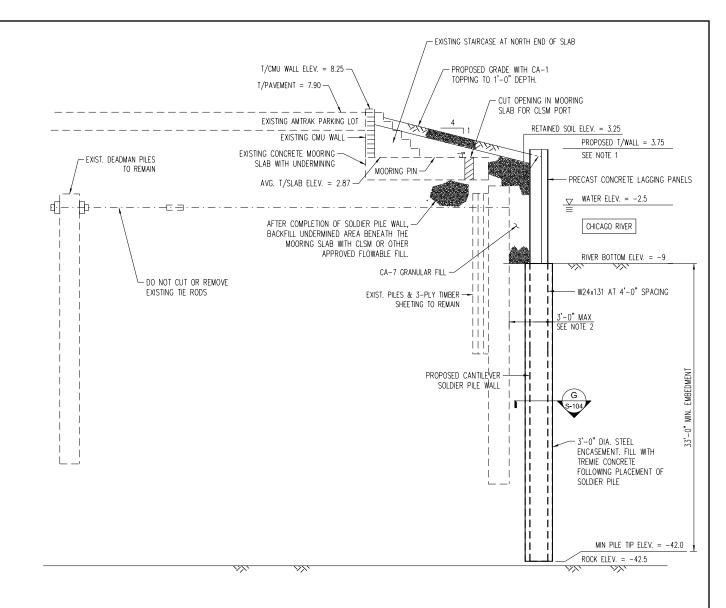


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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1
PROPOSED PLAN III

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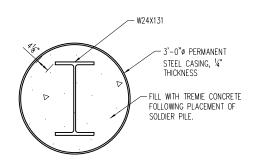
A SECTION THRU PARKING LOT S-104 SCALE:3/16" = 1'-0"



B SECTION THRU MOORING SLAB SCALE:3/16" = 1'-0"

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 WALL TO FRONT FACE OF NEW WALL.
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 WALL ELEMENTS TO ENSURE LESS THAN 3'-0"
 DIMENSION BETWEEN FACE OF EXISTING WALL
 AND FACE OF COMPLETED NEW WALL IN ALL
 LOCATIONS.



G SECTION THRU DRILLED SOLDIER PILE SCALE:3/4" = 1'-0"

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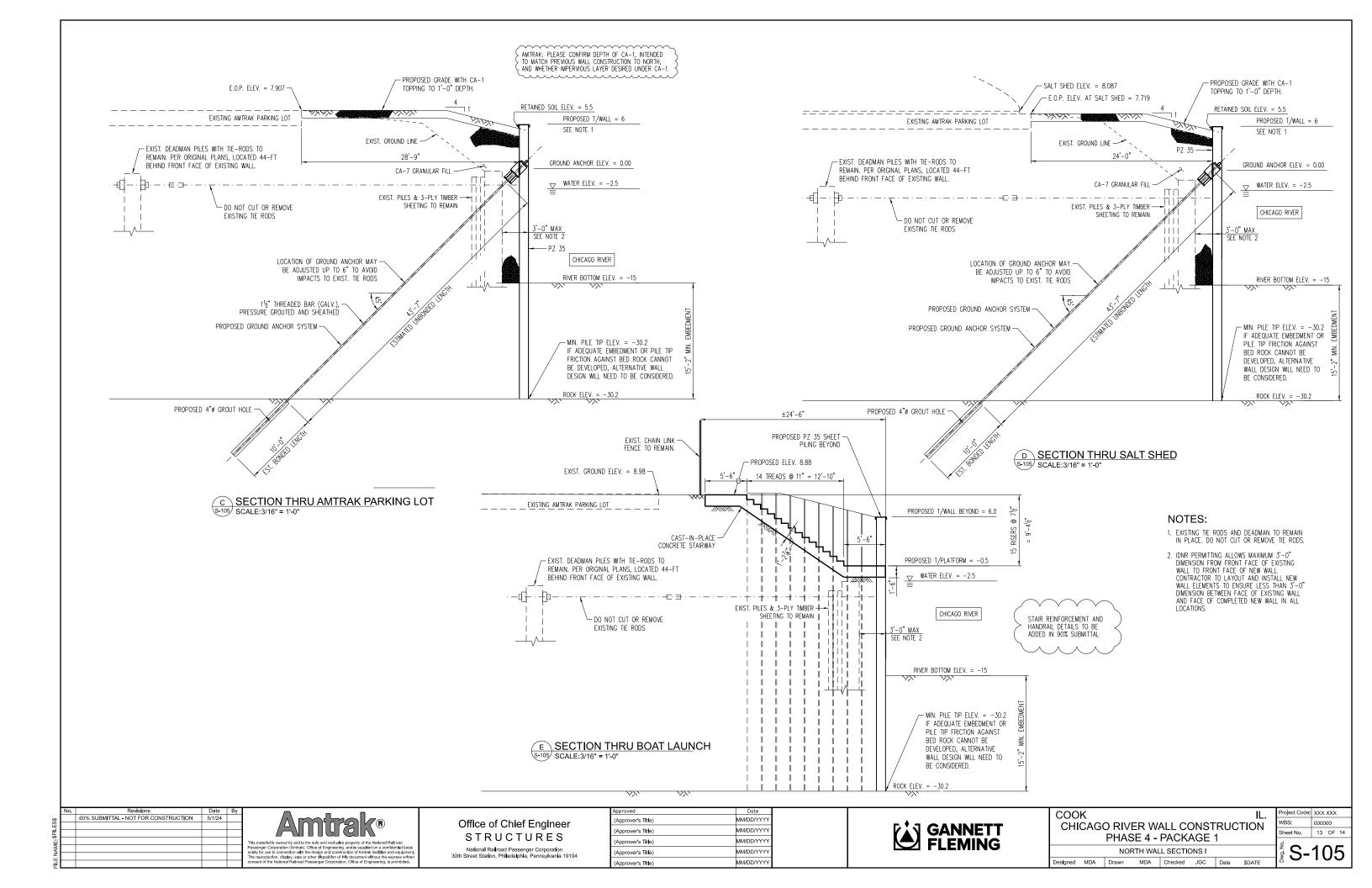
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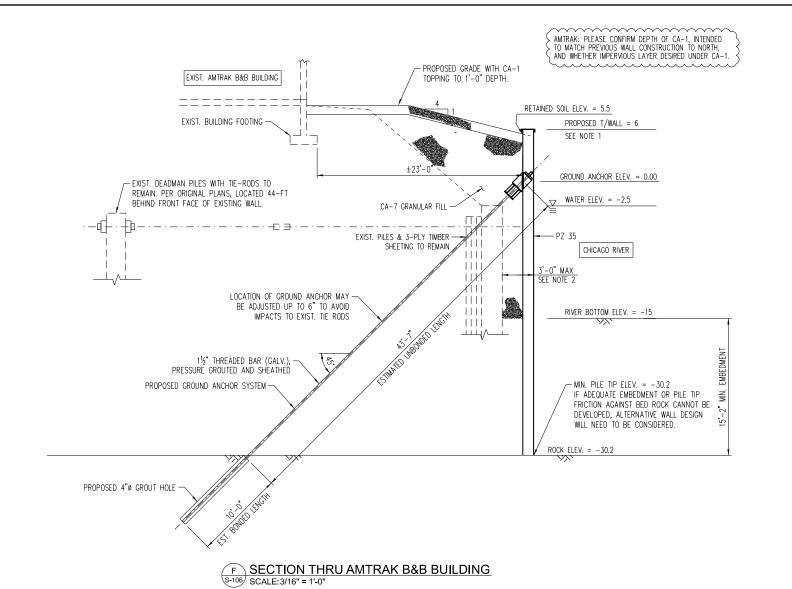


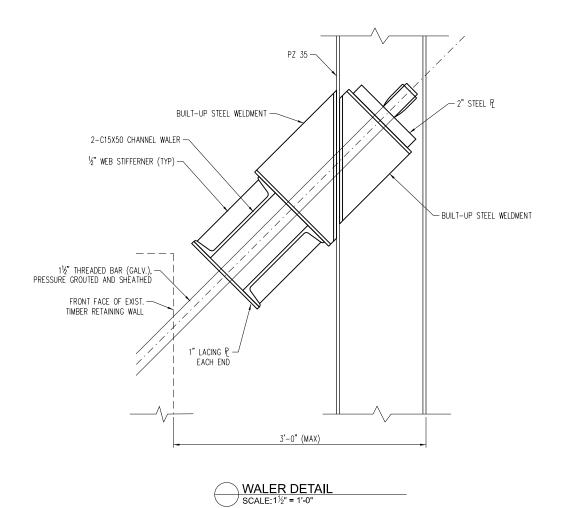
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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 1
COLITH WALL SECTIONS

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Sheet No. 12 OF 14

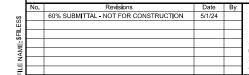






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 AND FACE OF COMPLETED NEW WALL IN ALL
 LOCATIONS.





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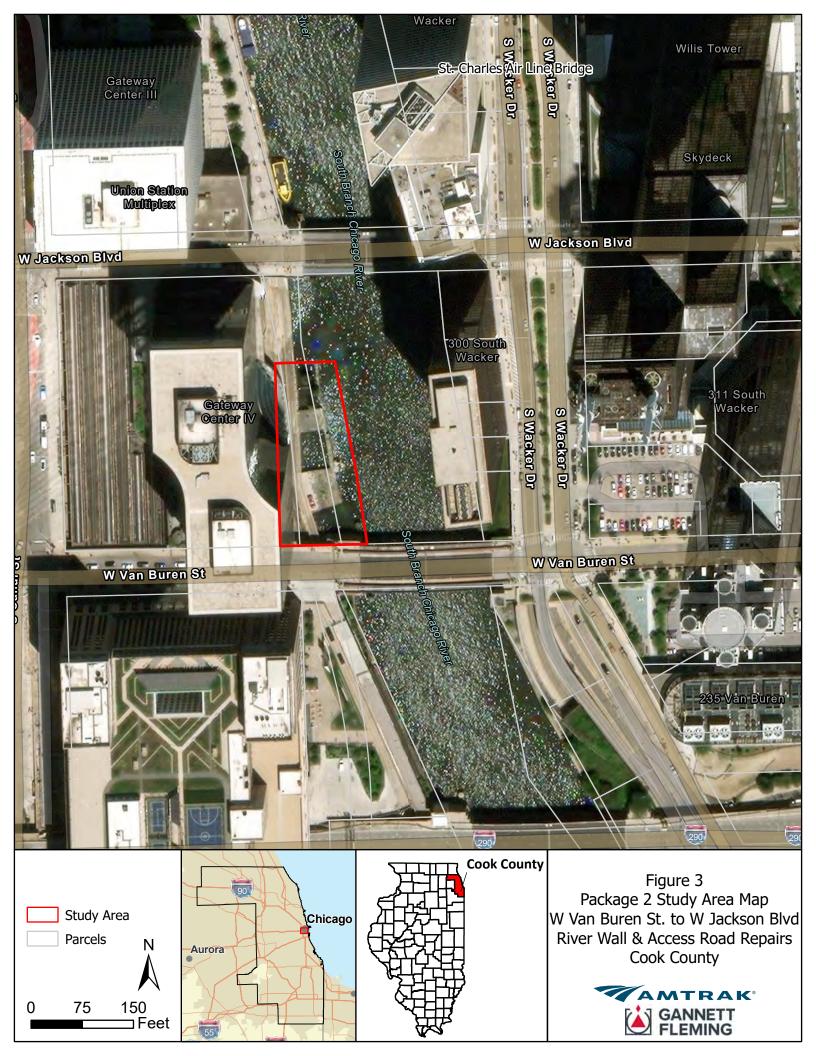
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CHICAGO RIVER WALL CONSTRUCTION	
PHASE 4 - PACKAGE 1	

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NORTH WALL SECTIONS II

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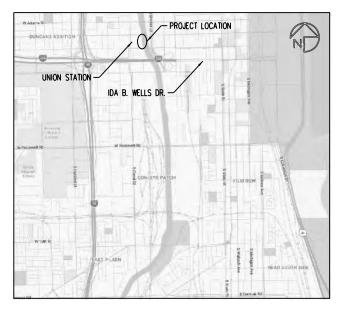




CHICAGO RIVER WALL CONSTRUCTION PHASE 4

CHICAGO, IL PACKAGE 2

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DWG. NO.	SHT. NO.	DRAWING TITLE
X-200	1	COVER SHEET
G-201	2	GENERAL NOTES AND ESTIMATED QUANTITIES
C-201	3	SITE PLAN I
C-202	4	SITE PLAN II
S-201	9	PROPOSED PLAN I
S-202	10	PROPOSED PLAN II

PROJECT SCOPE SUMMARY

ALONG THE WEST BANK OF THE SOUTH BRANCH OF THE CHICAGO RIVER, BETWEEN W. VAN BUREN ST. AND W. JACKSON BLVD. IS AN EXISTING ROAD ALLOWING ACCESS TO CHICAGO UNION STATION. THE ACCESS ROAD IS BUILT ABOVE AND ADJACENT TO EXISTING MASONRY, CONCRETE AND STEEL SHEET PILE RIVER WALLS. IN ADDITION, AN ABANDONED STEEL BEAM AND CONCRETE ARCH DECK STRUCTURE EAST OF THE EXISTING RIVER WALL, PREVIOUSLY UTILIZED BY THE CTA (CHICAGO TRANSIT AUTHORITY) AND ITS PREDECESSOR, NOW PARTIALLY SUPPORTS A TURNAROUND/PARKING AREA ADJACENT TO THE ACCESS ROAD. THE EXISTING RIVER WALLS, ARCH DECK STRUCTURE AND ITS SUBSTRUCTURE ARE IN VARYING STATES OF DISREPAIR AND POTENTIALLY CONTRIBUTING TO OCCASSIONAL FLOODING NEAR TRACK 40 AT CHICAGO UNION STATION. REPAIRS WILL BE MADE TO SELECT SECTIONS OF THE EXISTING CONCRETE AND MASONRY RIVER WALLS AND TO THE CONCRETE SUBSTRUCTURE ELEMENTS TO IMPROVE THEIR FUNCTIONALITY AND BRING THEM TO A STATE OF GOOD REPAIR. ADDITIONALLY, CONCRETE CURB AND GUARDRAIL REPAIRS WILL BE MADE IN AREAS ADJACENT TO THE ACCESS ROAD.

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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 2

Project Code: XXX XXX

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Sheet No. 1 OF 6

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signed	JGC	Drawn	ACB	Checked	JJH	Date	\$DATE

GENERAL NOTES:

 ALL REQUIREMENTS SHOWN ON THESE DRAWINGS SHALL BE ACCOMPLISHED AS SPECIFIED IN AMTRAK EP-3014 SPECIFICATIONS AND THE MOST CURRENT EDITION OF THE AMERICAN RAILWAY ENGINEERING AND MAINTENANCE-OF-WAY ASSOCIATION (AREMA) MANUAL FOR RAILWAY ENGINEERING.

CHAPTER 8 CONCRETE STRUCTURES AND FOUNDATIONS

CHAPTER 15 STEEL STRUCTURES

- ESTIMATED QUANTITIES INDICATED ARE APPROXIMATE BASED ON PLAN
 REQUIREMENTS AND DO NOT INCLUDE ANY ADDITIONAL REQUIREMENTS WHICH MAY
 BE DETERMINED BY THE ENGINEER IN FIELD.
- DIMENSIONS AND EXISTING CONDITIONS SHALL BE FIELD VERIFIED PRIOR TO THE FABRICATIONS OF ANY MATERIAL. IF DRAWINGS AND SPECIFICATIONS CONFLICT, THE DRAWINGS SHALL GOVERN IN FIELD.
- 4. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING STRUCTURES HAVE BEEN TAKEN FROM EXISTING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THESE DIMENSIONS AND SUBMIT ANY DEVIATIONS WITH SUPPORTING SURVEY NOTES AND DRAWINGS FOR REVIEW AND APPROVAL. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR CHANGES IN SCOPE OF WORK. CONTRACTOR WILL BE PAID FOR ACTUAL APPROVED QUANTITY IN THE FIELD. THIS SHALL BE DONE BEFORE THE COMMENCEMENT OF CONSTRUCTION AND PREPARATION OF THE SHOP DRAWINGS.
- 5. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

RIVER WALL NOTES:

- 1. THE CONTRACTOR SHALL COORDINATE ALL CONSTRUCTION ACTIVITIES WITH AMTRAK.
- STRUCTURAL CONCRETE MUST HAVE A MINIMUM COMPRESSIVE STRENGTH OF 4,000 PSI. CONCRETE SHALL BE CLASS DS PER IDOT SPECIFICATIONS.
- 3. ALL HOT ROLLED STEEL SHEET PILING SHALL BE ASTM A690 GR. 50 KSI STEEL.
- THE CONTRACTOR IS RESPONSIBLE FOR THE FULL COST TO REPAIR AND RESTORE UTILITIES DAMAGED BY HIS OPERATION.
- THE CONTRACTOR SHALL MAKE HIS OWN INVESTIGATION TO DETERMINE THE EXISTENCE, NATURE AND EXACT LOCATION OF ALL UTILITY LINES AND APPURTENANCES WITHIN THE LIMITS OF THE WORK. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 6. CONTRACTOR SHALL PROTECT AND/OR RELOCATE ALL UTILITIES, WHICH WOULD BE AFFECTED BY THE CONSTRUCTION. PIPES/DUCTS, FITTINGS, AND INSERTS ARE TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO THE MANUFACTURER'S RECOMMENDATION AND OWNERS REQUIREMENTS. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 7. THE CONTRACTOR SHALL EXERCISE CAUTION DURING ALL CONSTRUCTIONS OPERATIONS TO PREVENT ANY DAMAGE TO ADJACENT STRUCTURES, UTILITIES, AND BUILDING COMPONENTS NOT WITHIN THE SCOPE OF THIS WORK. OTHER MEMBERS, STRUCTURES, UTILITIES, AND STRUCTURAL COMPONENTS NOT WITHIN THE SCOPE OF THESE IMPROVEMENTS THAT ARE DAMAGED DURING THE CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER.
- 8. THE CONTRACTOR SHALL ESTABLISH SURVEY POINTS, BENCH MARKS AND ALL NECESSARY CONTROL POINTS TO CONSTRUCT THE PROPOSED PROJECT AND ITS INDIVIDUAL ELEMENTS. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.

RIVER WALL NOTES (CONT.):

- 9. ALL WORK SHALL BE PERFORMED SO AS NOT TO INTERFERE WITH THE FREE FLOW OF NAVIGATION AND SUCH THAT THE NAVIGABLE DEPTHS AND CLEARANCES WITHIN THE CANAL ARE NOT IMPAIRED. ANY WORK EQUIPMENT/BARGES LOCATED WITHIN THE WATERWAY SHALL BE MOVABLE AT ANY TIME, AND THE CONTRACTOR SHALL NOTIFY AND RECEIVE APPROVAL FROM THE U.S. COAST GUARD AS TO MOORING THE BARGES AT THE WORK SITE. ANY WORK BARGE OR OTHER EQUIPMENT IN THE NAVIGABLE CHANNEL BE IDENTIFIED WITH THE NECESSARY LIGHTS AND SIGNALS REQUIRED BY "INLAND NAVIGATIONAL RULES". THE LOCATION AND TIMES OF PLACEMENT OF EQUIPMENT IN THE CANAL SHALL BE REPORTED TO AND APPROVED BY ALL OF THE NECESSARY AUTHORITIES.
- 10. ALL DEBRIS RESULTING FROM THE REMOVAL OF THE EXISTING STRUCTURES OR CREATED WHILE PERFORMING THE SPECIFIED WORK SHALL BE REMOVED FROM THE PROJECT SITE. NO MATERIAL SHALL BE DISCHARGED INTO THE WATER/RIVER. THE CONTRACTOR SHALL IMPLEMENT PROTECTIVE MEASURES SO AS TO PREVENT ANY DEBRIS FROM FALLING INTO THE CHICAGO RIVER. IF ANY DEBRIS FALLS INTO THE WATERWAY, THE CONTRACTOR SHALL REMOVE IT FROM THE RIVER AT HIS/HER EXPENSE AND TO THE ENGINEER'S SATISFACTION. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT
- 11. DEBRIS AND HAZARDOUS SUBSTANCES SHALL BE DISPOSED OF IN ACCORDANCE WITH THE REGULATIONS OF ALL GOVERNING AGENCIES. THE REMOVAL OF HAZARDOUS SUBSTANCES AND OBTAINING THE PERMITS FOR SUCH DISPOSAL SHALL BE SOLE REPSONSIBILITY OF THE CONTRACTOR. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 12. ALL ELEVATIONS REFER TO CHICAGO CITY DATUM.
- 13. AFTER CONSTRUCTION HAS BEEN COMPLETED ALL EXCESS MATERIALS AND TRASH SHALL BE REMOVED AND DISPOSED OF AT AN OFF-SITE WASTE FACILITY. THE SITE SHALL BE BROOM CLEANED AND WASHED DOWN LEAVING THE PREMISES IN A CLEAN CONDITION AND SAFE CONDITION. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 14. PROPOSED SHEET PILE WALL SHALL BE CONSTRUCTED IN PHASES. THE NUMBER AND LIMITS OF PHASES WILL BE DETERMINED BY AMTRAK PRIOR TO START OF CONSTRUCTION
- 15. CONTRACTOR SHALL REFER TO AMTRAKCONTRACTOR.COM FOR REQUIREMENTS AND DETAILS REGARDING EMPLOYEE SCREENING REQUIREMENTS.

SITE SECURITY & TRAFFIC MANAGEMENT:

- CONTRACTOR IS RESPONSIBLE FOR SITE SECURITY DURING CONSTRUCTION, ESPECIALLY DURING NON-WORK HOURS.
- 2. CONTRACTOR SHALL REPLACE ALL SITE SECURITY COMPONENTS TO MATCH THE EXISTING LOCATIONS, SUCH AS BUT NOT LIMITED TO, CHAINLINK FENCES, GATES, GUARDRAILS, POLES, AND ALL UTILITIES AND ELECTRICAL EQUIPMENT THAT IS ASSOCIATED WITH THE SECURITY COMPONENTS. COMPONENTS NOT WITHIN THE SCOPE OF THESE IMPROVEMENTS THAT ARE DAMAGED DURING THE CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER.

TOTAL BILL OF MATERIAL

TOTAL BILL OF WATERIAL					
ITEM	UNIT	QUANTITY			
CIVIL SITE WORK	L. SUM				
FORMED CONCRETE REPAIR (VERTICAL FACE)	S.F.				
ADHESIVE ANCHORING OF HORIZONTAL BARS, 12 INCH	EACH				
STRUCTURAL CRACK REPAIR	L.F.				
FORMED CONCRETE REPAIR (HORIZONTAL FACE)	S.F.				
ADHESIVE ANCHORING OF VERTICAL BARS, 12 INCH	EACH				
CONCRETE REMOVAL	C.Y.				
TIMBER FENDER REMOVAL	L.F.				

* THE QUANTITIES PROVIDED ABOVE ARE FOR BIDDING PURPOSES ONLY. THESE QUANTITIES MUST BE ADJUSTED IN THE FIELD BY THE ENGINEER AS CONDITIONS WARRANT.



Office of Chief Engineer STRUCTURES

National Railroad Passenger Corporation 30th Street Station, Philadelphia, Pennsylvania 19104

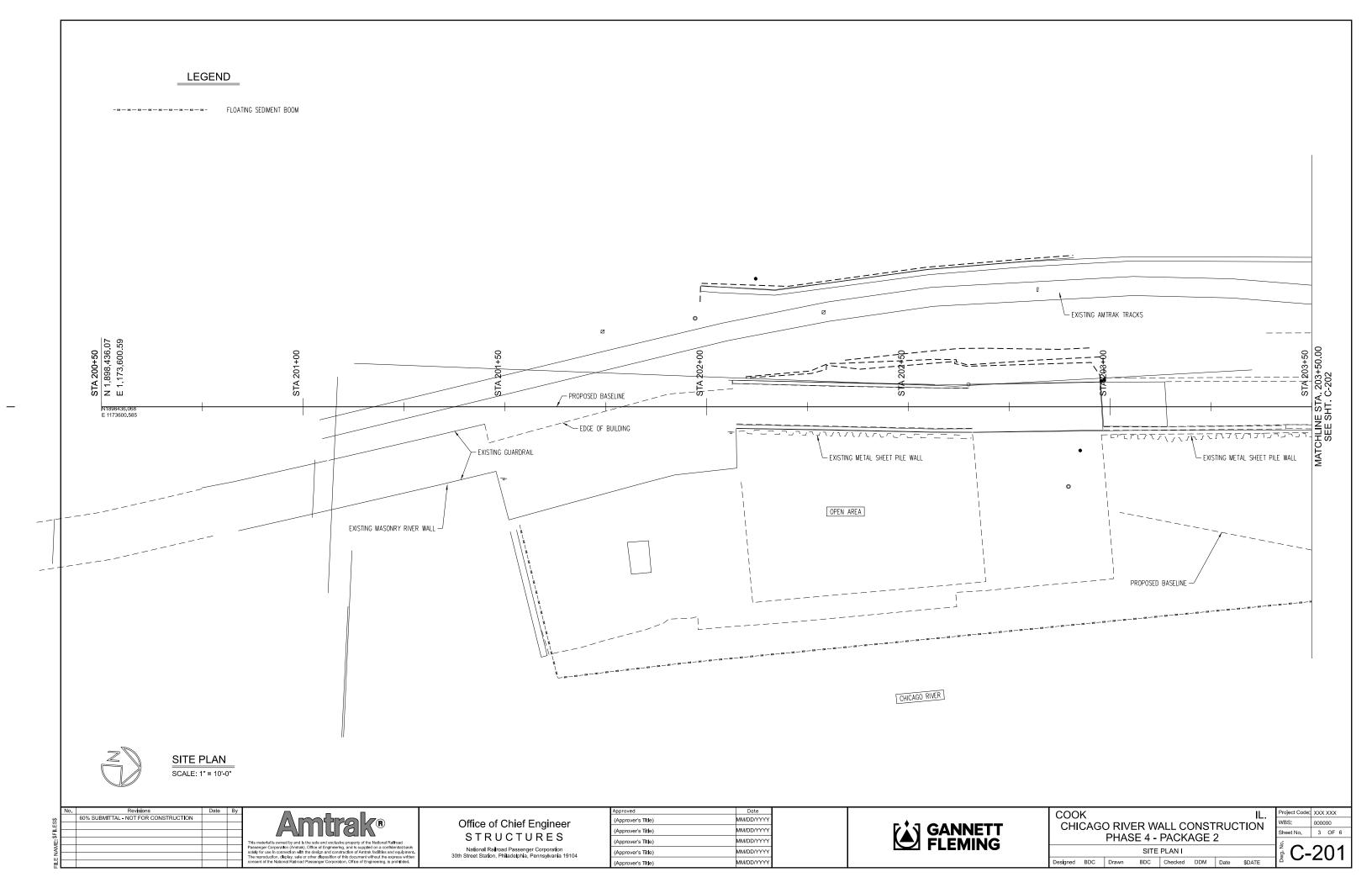
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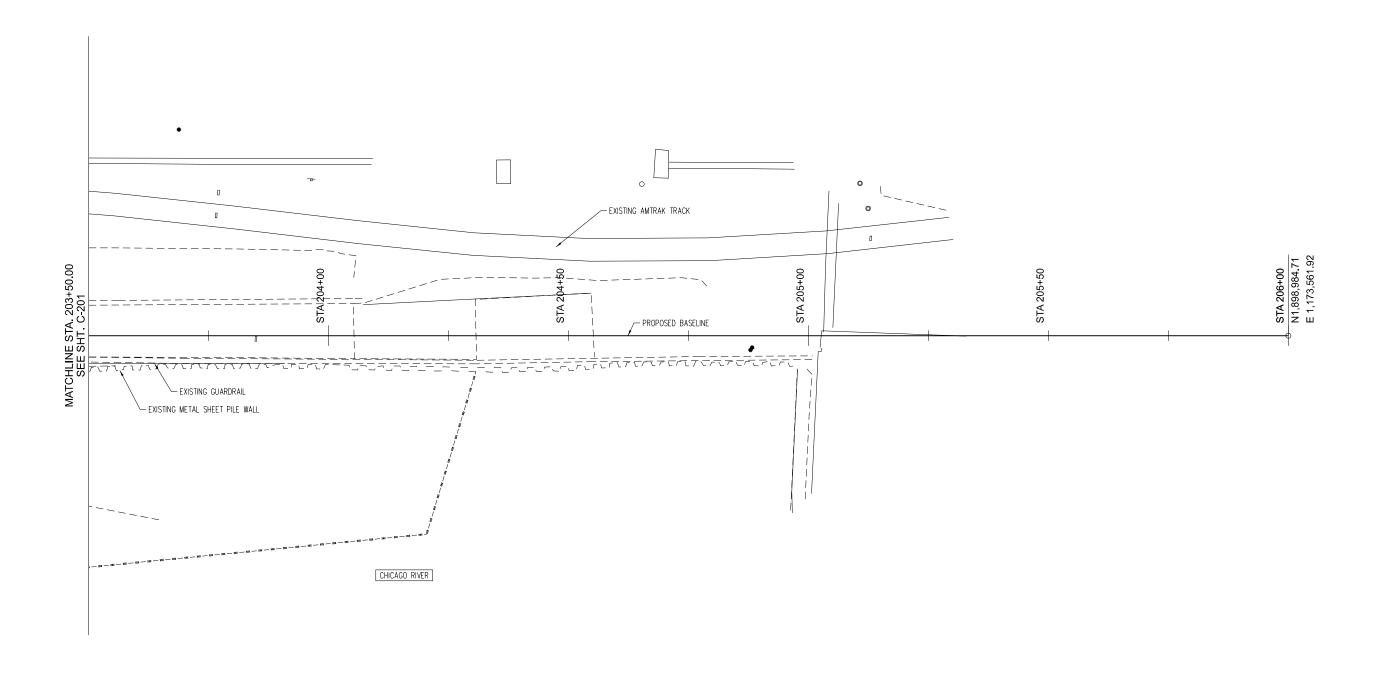


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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 2
GENERAL NOTES AND ESTIMATED QUANTITIES

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Project Code: XXX XXX WBS: 000000 Sheet No. 2 OF 6







SITE PLAN
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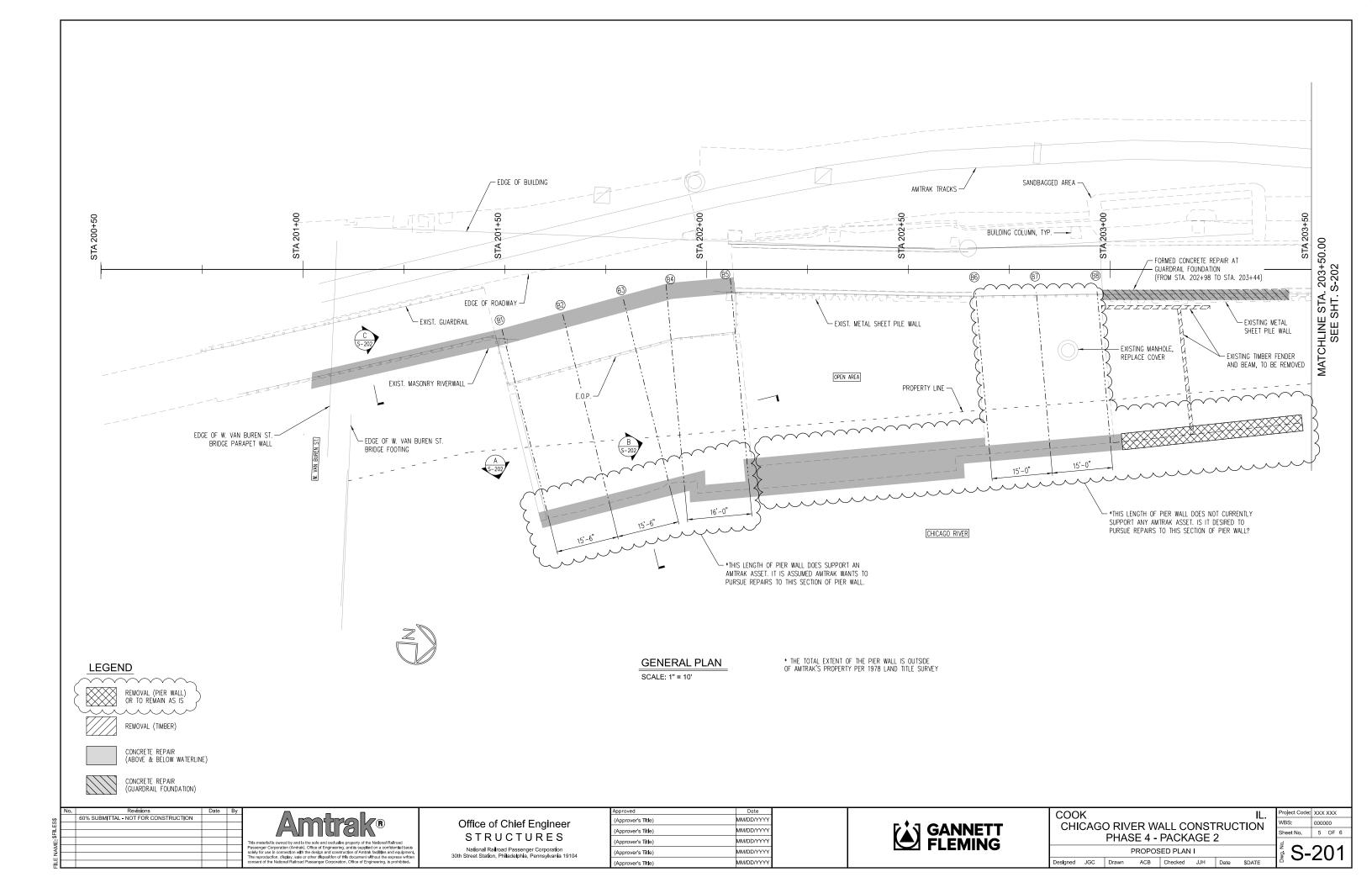
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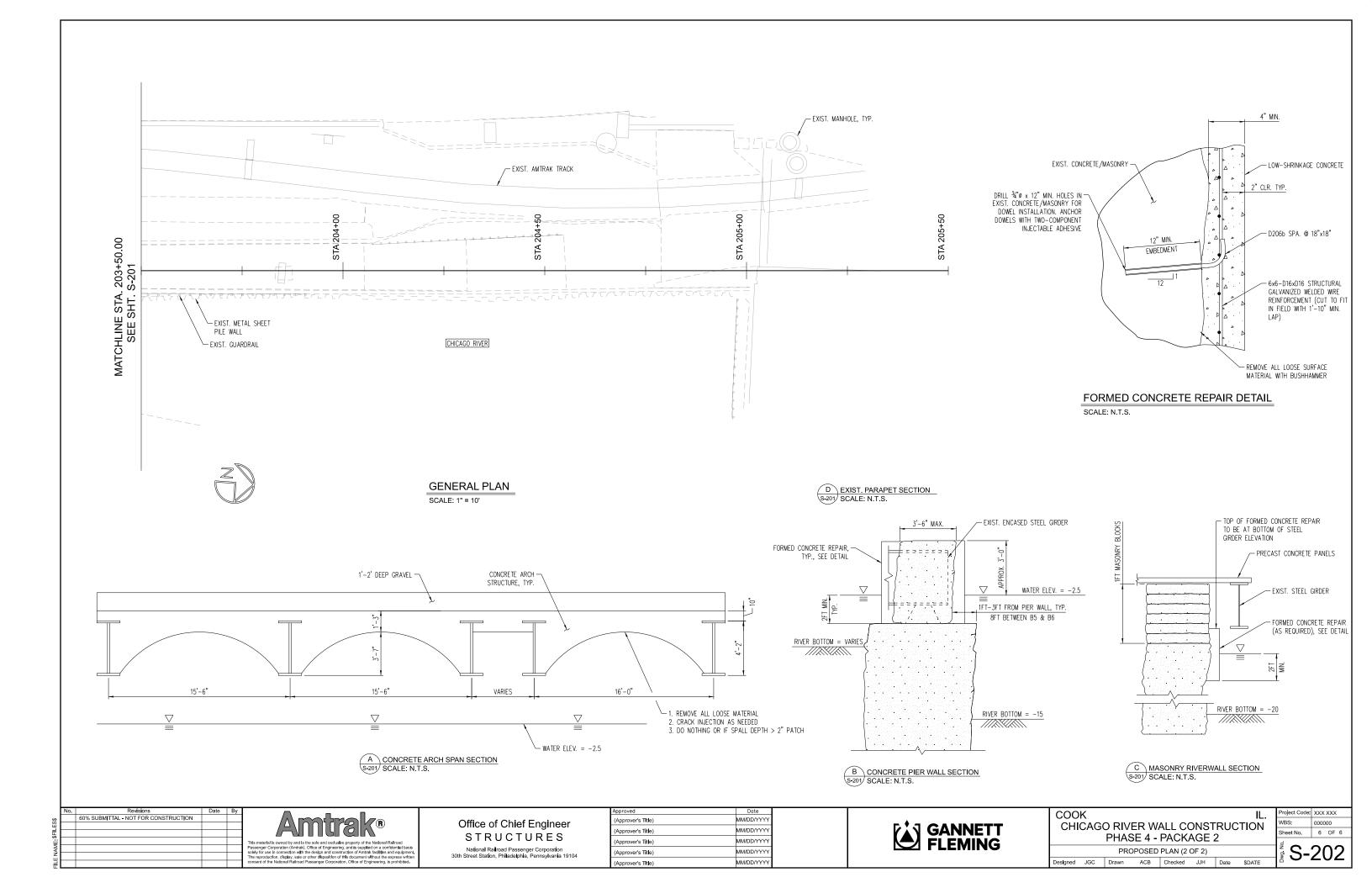


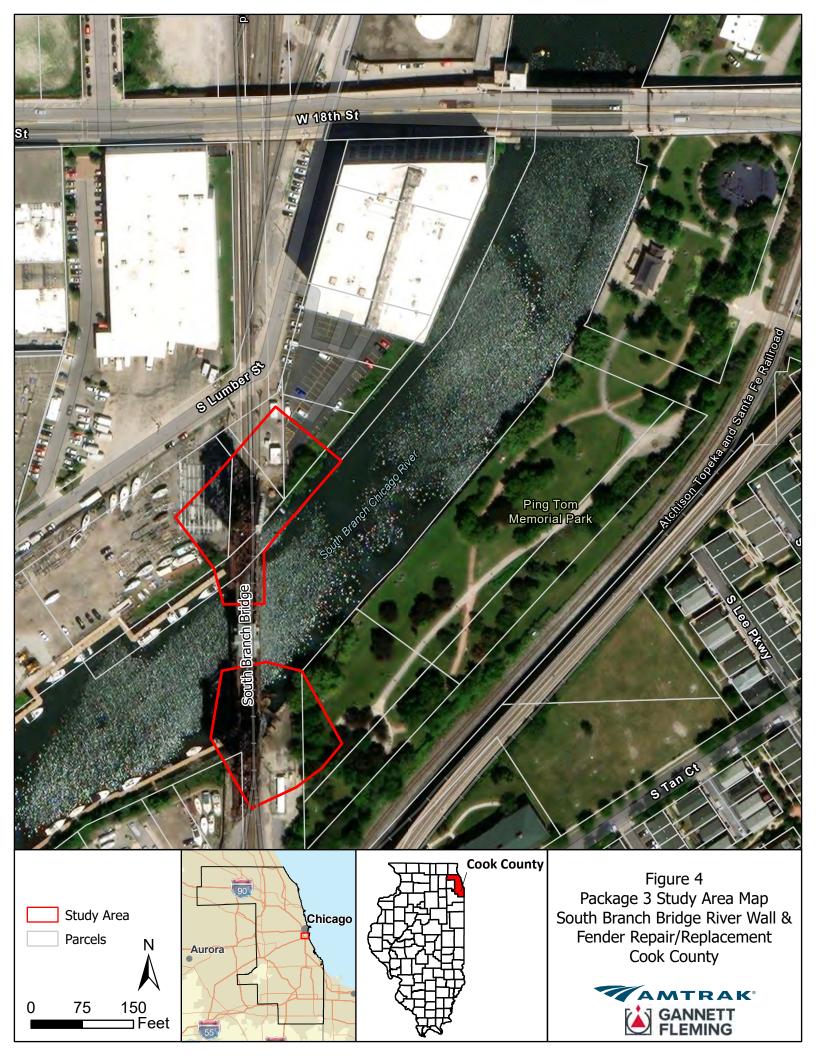
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CHICAGO RIVER WALL CONSTRUCTION
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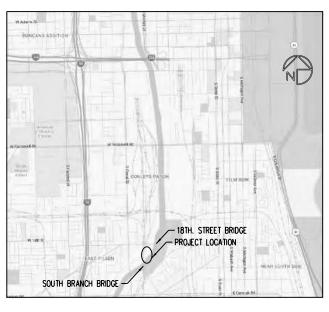




CHICAGO RIVER WALL CONSTRUCTION PHASE 4

CHICAGO, IL PACKAGE 3

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X-300	1	COVER SHEET
G-301	2	GENERAL NOTES AND ESTIMATED QUANTITIES
C-301	3	SITE PLAN I
C-302	4	SITE PLAN II
C-303	5	SITE PLAN III
C-304	6	RESTORATION PLAN I
C-305	7	RESTORATION PLAN II
C-306	8	RESTORATION PLAN III
S-301	9	OVERALL PLAN
S-302	10	NORTH BANK PROPOSED PLAN
S-303	11	SOUTH BANK PROPOSED PLAN
S-304	12	SECTIONS
S-305	13	FENDER SYSTEMS — GENERAL PLAN
S-306	14	NORTH FENDER SYSTEM REPAIRS
S-307	15	SOUTH FENDER SYSTEM REPAIRS
S-308	16	PILE SPLICING DETAILS
S-309	17	WALER LAP AND PILE FASTENING DETAIL
S-310	18	DOLPHIN STEEL CAP DETAILS

PROJECT SCOPE SUMMARY

THE SOUTH BRANCH BRIDGE IS A VERTICAL LIFT BRIDGE THAT SPANS THE SOUTH BRANCH OF THE CHICAGO RIVER IN CHICAGO, IL. THE BRIDGE IS FIVE SPANS CONSISTING OF A LIFT SPAN, TWO TOWER SPANS, AND TWO APPROACH SPANS. BOTH THE NORTH AND SOUTH TOWER SPANS ARE PROTECTED BY AN EXISTING TIMBER FENDER SYSTEM, WHICH HAS SIGNS OF DETERIORATION. ADDITIONALLY, THERE ARE EXISTING SEGMENTS OF RIVER WALL FROM W. 18TH ST. DOWN TO THE SOUTH BRANCH BRIDGE ALONG THE WEST BANK OF THE SOUTH BRANCH OF THE CHICAGO RIVER, JUST NORTH OF THE SOUTH BRANCH BRIDGE IS A SEGMENT OF EXISTING TIMBER RIVER WALL WITH SIGNS OF DETERIORATION. ELEMENTS OF THE FENDER SYSTEM SHOWING ADVANCED DETERIORATION WILL BE REPLACED IN KIND OR REPAIRED. THE EXISTING TIMBER DOLPHINS WILL ALSO HAVE CAPS INSTALLED TO PROTECT AGAINST FURTHER DETERIORATION. THE SEGMENT OF RETAINING WALL ALONG THE WEST BANK OF THE CHICAGO RIVER NORTH OF THE SOUTH BRANCH BRIDGE WILL BE REPLACED WITH A NEW CANTILLEVERED STEEL SOLDIER PILE WALL WITH PRECAST CONCRETE LAGGING PANELS. ADDITIONALLY, A NEW SEGMENT OF STEEL SHEET PILE WALL WILL EITHER BE INSTALLED ALONG THE AMTRAK PROPERTY LINE DUE NORTH FROM THE WEST BANK OF THE RIVER OR CONTINUE TO THE NORTHEAST APPROXIMATELY 80-PT ALONG THE WEST BANK OF THE RIVER OR CONTINUE TO THE TERMINATING AT AN EXISTING STEEL SHEET PILE WALL, WHICH IS NOT OWNED OR MAINTAINED BY AMTRAK. IN THE SOUTHEAST QUADRANT OF THE SOUTH BRANCH BRIDGE, AN EXISTING STECKED TIMBER TIE RETAINING WALL SUPPORTING THE EMBANKMENT AROUND AN EXISTING SIGNAL HOUSE WILL ALSO BE REPLACED.

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CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 3

COVER SHEET

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WBS: 000000 Sheet No. 1 OF 16

GENERAL NOTES:

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CHAPTER 15 STEEL STRUCTURES

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- 3. ALL HOT ROLLED STEEL SHEET PILING SHALL BE ASTM A690 GR. 50 KSI STEEL.
- THE CONTRACTOR IS RESPONSIBLE FOR THE FULL COST TO REPAIR AND RESTORE UTILITIES DAMAGED BY HIS OPERATION.
- THE CONTRACTOR SHALL MAKE HIS OWN INVESTIGATION TO DETERMINE THE EXISTENCE, NATURE AND EXACT LOCATION OF ALL UTILITY LINES AND APPURTENANCES WITHIN THE LIMITS OF THE WORK, ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 6. CONTRACTOR SHALL PROTECT AND/OR RELOCATE ALL UTILITIES, WHICH WOULD BE AFFECTED BY THE CONSTRUCTION. PIPES/DUCTS, FITTINGS, AND INSERTS ARE TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO THE MANUFACTURER'S RECOMMENDATION AND OWNERS REQUIREMENTS. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 11. THE CONTRACTOR SHALL EXERCISE CAUTION DURING ALL CONSTRUCTIONS OPERATIONS TO PREVENT ANY DAMAGE TO ADJACENT STRUCTURES, UTILITIES, AND BUILDING COMPONENTS NOT WITHIN THE SCOPE OF THIS WORK. OTHER MEMBERS, STRUCTURES, UTILITIES, AND STRUCTURAL COMPONENTS NOT WITHIN THE SCOPE OF THESE IMPROVEMENTS THAT ARE DAMAGED DURING THE CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER.
- 12. THE CONTRACTOR SHALL ESTABLISH SURVEY POINTS, BENCH MARKS AND ALL NECESSARY CONTROL POINTS TO CONSTRUCT THE PROPOSED PROJECT AND ITS INDIVIDUAL ELEMENTS. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 13. THE U.S. ARMY CORPS OF ENGINEERS, THE ILLINOIS DEPARTMENT OF NATURAL RESOURCES, THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, AND HARBOR PERMITS FOR THE PROPOSED RIVER WALL WORK WILL BE SECURED BY AMTRAK AND THEIR REPRESENTATIVES. THE SECURING AND OBTAINING OF ALL OTHER NECESSARY CONSTRUCTION PERMITS FOR THE SPECIFIED WORK SHALL BE SOLE RESPONSIBILITY OF THE CONTRACTOR, INCLUDING THE CDOT OFFICE OF UNDERGROUND COORDINATION PERMIT.

RIVER WALL NOTES (CONT.):

- 14. ALL WORK SHALL BE PERFORMED SO AS NOT TO INTERFERE WITH THE FREE FLOW OF NAVIGATION AND SUCH THAT THE NAVIGABLE DEPTHS AND CLEARANCES WITHIN THE CANAL ARE NOT IMPAIRED. ANY WORK EQUIPMENT/BARGES LOCATED WITHIN THE WATERWAY SHALL BE MOVABLE AT ANY TIME, AND THE CONTRACTOR SHALL NOTIFY AND RECEIVE APPROVAL FROM THE U.S. COAST GUARD AS TO MOORING THE BARGES AT THE WORK SITE. ANY WORK BARGE OR OTHER EQUIPMENT IN THE NAVIGABLE CHANNEL BE IDENTIFIED WITH THE NECESSARY LIGHTS AND SIGNALS REQUIRED BY "INLAND NAVIGATIONAL RULES". THE LOCATION AND TIMES OF PLACEMENT OF EQUIPMENT IN THE CANAL SHALL BE REPORTED TO AND APPROVED BY ALL OF THE NECESSARY AUTHORITIES.
- 15. ALL DEBRIS RESULTING FROM THE REMOVAL OF THE EXISTING STRUCTURES OR CREATED WHILE PERFORMING THE SPECIFIED WORK SHALL BE REMOVED FROM THE PROJECT SITE. NO MATERIAL SHALL BE DISCHARGED INTO THE WATER/RIVER. THE CONTRACTOR SHALL IMPLEMENT PROTECTIVE MEASURES SO AS TO PREVENT ANY DEBRIS FROM FALLING INTO THE CHICAGO RIVER. IF ANY DEBRIS FALLS INTO THE WATERWAY, THE CONTRACTOR SHALL REMOVE IT FROM THE RIVER AT HIS/HER EXPENSE AND TO THE ENGINEER'S SATISFACTION. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 16. DEBRIS AND HAZARDOUS SUBSTANCES SHALL BE DISPOSED OF IN ACCORDANCE WITH THE REGULATIONS OF ALL GOVERNING AGENCIES. THE REMOVAL OF HAZARDOUS SUBSTANCES AND OBTAINING THE PERMITS FOR SUCH DISPOSAL SHALL BE SOLE REPSONSIBILITY OF THE CONTRACTOR. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- 17. ALL ELEVATIONS REFER TO CHICAGO CITY DATUM.
- 18. AFTER CONSTRUCTION HAS BEEN COMPLETED ALL EXCESS MATERIALS AND TRASH SHALL BE REMOVED AND DISPOSED OF AT AN OFF-SITE WASTE FACILITY. THE SITE SHALL BE BROOM CLEANED AND WASHED DOWN LEAVING THE PREMISES IN A CLEAN CONDITION AND SAFE CONDITION. ALL COST FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THIS CONTRACT.
- PROPOSED SHEET PILE WALL SHALL BE CONSTRUCTED IN PHASES. THE NUMBER AND LIMITS OF PHASES WILL BE DETERMINED BY AMTRAK PRIOR TO START OF CONSTRUCTION.
- 20. CONTRACTOR SHALL REFER TO AMTRAKCONTRACTOR.COM FOR REQUIREMENTS AND DETAILS REGARDING EMPLOYEE SCREENING REQUIREMENTS.

SITE SECURITY & TRAFFIC MANAGEMENT:

- CONTRACTOR IS RESPONSIBLE FOR SITE SECURITY DURING CONSTRUCTION, ESPECIALLY DURING NON-WORK HOURS.
- 2. CONTRACTOR SHALL REPLACE ALL SITE SECURITY COMPONENTS TO MATCH THE EXISTING LOCATIONS, SUCH AS BUT NOT LIMITED TO, CHAINLINK FENCES, GATES, GUARDRAILS, POLES, AND ALL UTILITIES AND ELECTRICAL EQUIPMENT THAT IS ASSOCIATED WITH THE SECURITY COMPONENTS. COMPONENTS NOT WITHIN THE SCOPE OF THESE IMPROVEMENTS THAT ARE DAMAGED DURING THE CONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR AND TO THE SATISFACTION OF THE ENGINEER.

SOIL MANAGEMENT:

1. CONTRACTOR SHALL IMMEDIATELY NOTIFY AMTRAK UPON OBSERVATION OF ENVIRONMENTAL CONTAMINATION (E.G. PETROLEUM LIKE ODORS, STAINING OR FREE PRODUCT). CONTAMINATED SOILS SHALL BE STOCKPILED SEPARATELY FROM OTHER SOILS. CONTAMINATE SOILS SHALL BE STOCKPILED ON EITHER 6-MIL POLY OR AN IMPERMEABLE SURFACE SUCH AS ASPHALT OR CONCRETE. THE CONTAMINATED SOIL STOCKPILE SHALL BE SECURELY COVERED WITH 6-MIL POLYETHYLENE SHEETING AT THE END OF EACH WORK DAY. SHEETING SHALL BE SECURELY WEIGHTED DOWN. SOILS THAT DO NOT EXHIBIT CONTAMINATION ARE SUITABLE FOR REUSE BY THE CONTRACTOR AS BACKFILL. ALL [EXCESS] SOIL WILL BE TESTED AND DISPOSED BY AMTRAK. REFER TO SECTION [026100] IN THE CONTRACTOR SPECIFICATIONS FOR "REMOVAL AND DISPOSAL OF CONTAMINATED SOILS".

STRUCTURAL STEEL:

ANCHOR RODS

1. MATERIAL SHALL CONFORM TO THE FOLLOWING REQUIREMENTS:

ALL STEEL, U.N.O. A709 GR 50 OR A992
SHEET PILING A690 GR 50
HANDRAIL HSS STEEL ASTM A500 GRADE C
BOLTS ASTM F3125 GR A325 TYPE 1
M. BOLTS ASTM A307

 DESIGN, FABRICATION AND ERECTION PER THE AREMA MANUAL FOR RAILWAY ENGINEERING, CHAPTER 15, STEEL STRUCTURES. DESIGN PER 2023 EDITION (EXCEPT AS OTHERWISE NOTED), FABRICATION AND ERECTION PER CURRENT EDITION.

ASTM F1554, GR. 55

- 3. BOLTS SHALL INCLUDE COMPATIBLE HEAVY HEX NUTS AND FLAT WASHERS.
- 4. ALL BOLTS SHALL BE $^{7}\!6"$ DIA. H.S. GALVANIZED BOLTS IN $^{15}\!6"$ DIA. OPEN HOLES, UNLESS OTHERWISE NOTED.

ALL BOLTS SHALL BE INSTALLED IN ACCORDANCE WITH PROJECT SPECIFICATIONS AND AREMA CHAPTER 15, SECTION 3.2. ALL BOLTED CONNECTIONS SHALL BE SLIP-CRITICAL.

- ALL FAYING SURFACES, REGARDLESS OF LOCATION, SHALL BE CLEANED TO A MINIMUM OF SSPC-SP 6, COMMERCIAL BLAST CLEANING.
- 6. CLEAN SURFACE TO BE WELDED FROM ALL PAINT AND GALVANIZED COATING THAT IS IN THE VICINITY OF THE WELD BY GRINDING WITH POWER TOOLS, IN ACCORDANCE WITH SSPC-SP 15. IF ADDITIONAL RESIDUE FROM THE PAINT REMAINS, A SOLVENT CLEANER MAY BE USED TO REMOVE IT.
- 7. ONCE COMPLETE, APPLY A COLD GALVANIZING COMPOUND SPRAY OR INORGANIC ZINC PAINT TO PROVIDE CORROSION PROTECTION TO THE AREAS THAT WERE CLEANED AND WELDED. ALL PAINTING PROCEDURES SHALL BE IN ACCORDANCE WITH AMTRAK EP-3008 SECTION 09910.

8. WELDING:

- A. ALL WELDING SHALL BE IN ACCORDANCE WITH AWS D1.5, BRIDGE WELDING CODE.
- B. ALL FIELD WELDING SHALL BE WITH THE SHILEDED METAL ARC WELDING (SMAW) OR FLUX-CORED ARC WELDING (FCAW) PROCESS. WELDING ELECTRODES MUST BE E7018 FOR SMAW OR E70T-1, 5 FOR FCAW.
- C. ALL OTHER WELDING SHALL BE WITH THE SAW OR SMAW PROCESS.
- D. WELDERS MUST POSSESS VALID CERTIFICATION.
- E. FIELD WELDING IS NOT PERMITTED UNLESS SHOWN ON THE PLANS OR APPROVED IN WRITING BY THE ENGINEER.
- ALL SURFACES OF NEW STEEL, EXCEPT AS NOTED HEREIN, SHALL BE CLEANED TO A MINIMUM OF SSPC-SP1, SOLVENT CLEANING.
- WHERE APPLICABLE, STRUCTURAL STEEL SHALL BE GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM A123 SPECIFICATIONS.

BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED AFTER FABRICATION IN ACCORDANCE WITH ASTM F2329 SPECIFICATIONS.

- STRUCTURAL STEEL SHALL BE METALIZED, SEALED AND TOP-COAT PAINTED PER THE SPECIFICATIONS.
- 12. FOR STRUCTURAL PAINT, PLEASE REFER TO AMTRAK EP3008 SECTION 09910 "SPECIFICATIONS FOR THE PROCUREMENT AND APPLICATION OF PAINT FOR STEEL STRUCTURES".

FENDER SYSTEM NOTES:

- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- 2. DUE TO THE MODIFICATIONS AND CHANGES TO THE STRUCTURE SINCE ITS ORGINAL CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE CORRECTNESS OF ALL REFERENCE DRAWINGS. PLAN DIMENSIONS, AND DETAILS RELATIVE TO THE EXISTING STRUCTURE HAVE BEEN TAKEN FROM FIELD MEASUREMENTS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND DETAILS IN THE FIELD AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. SUCH VARIATIONS SHALL NOT BE THE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY INSTALLED AT THE UNIT PRICE BID FOR THE WORK.

PACKAGE 3 BILL OF MATERIAL

ITEM	UNIT	QUANTITY
CIVIL SITE WORK	L. SUM	
FURNISHING SOLDIER PILES (W SECTION)	L.F.	
CONCRETE ENCASEMENT	C.Y.	
PERMANENT STEEL CASING	L.F.	
PRECAST CONCRETE LAGGING	S.F.	
TIMBER PILE REPLACEMENT	EA.	
TIMBER WALER REPLACEMENT	L.F.	
STEEL DOLPHIN CAP	EA.	

* THE QUANTITIES PROVIDED ABOVE ARE FOR BIDDING PURPOSES ONLY. THESE QUANTITIES MUST BE ADJUSTED IN THE FIELD BY THE ENGINEER AS CONDITIONS WARRANT.

FENDER SYSTEM NOTES (CONTINUED):

- 3. THE CONTRACTOR SHALL MAKE THEIR OWN INVESTIGATION TO DETERMINE THE EXISTENCE, NATURE, AND EXACT LOCATION OF ALL UTILITY LINES AND APPURTENANCES WITHIN THE LIMITS OF THE REHABILITATION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- 4. THE CONTRACTOR SHALL PROTECT AND/OR RELOCATE ALL UTILITIES, WHICH WOULD BE AFFECTED BY THE CONSTRUCTION. PIPES/DUCTS, FITTINGS, AND INSERTS ARE TO BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO THE MANUFACTURER'S RECOMMENDATION AND OWNER'S REQUIREMENTS. THE COST OF THIS WORK WILL BE CONSIDERED INCLUDED IN THE COST OF THE CONTRACT. THE CONTRACTOR IS RESPONSIBLE FOR THE FULL COST TO REPAIR AND RESTORE ANY AND ALL UTILITIES DAMAGED BY THEIR OPERATIONS.
- 5. THE CONTRACTOR SHALL EXERCISE CAUTION DURING ALL CONSTRUCTION OPERATIONS TO PREVENT ANY DAMAGE TO ADJACENT STRUCTURES AND COMPONENTS NOT WITHIN THE SCOPE OF THIS PROJECT. STRUCTURES AND COMPONENTS NOT WITHIN THE SCOPE OF THE RECONSTRUCTION AND IMPROVEMENTS THAT ARE DAMAGED DURING THE RECONSTRUCTION OPERATIONS SHALL BE REPAIRED OR REPLACED AT THE EXPENSE OF THE CONTRACTOR TO THE SATISFACTION OF THE CLIENT.
- 6. ALL DEBRIS RESULTING FROM THE REMOVAL OF THE EXISTING STRUCTURE OR CREATED WHILE PERFORMING THE SPECIFIED WORK SHALL BE REMOVED FROM THE PROJECT SITE. NO MATERIAL SHALL BE DISCHARGED INTO THE WATERWAY AROUND THE PROJECT SITE IN ACCORDANCE WITH COAST GUARD AND CITY OF CHICAGO REGULATIONS. THE CONTRACTOR SHALL IMPLEMENT PROTECTIVE MEASURES SO AS TO PREVENT ANY DEBRIS FROM FALLING INTO THE SOUTH BRANCH OF THE CHICAGO RIVER. IF ANY DEBRIS FALLS INTO THE WATERWAY, THE CONTRACTOR SHALL REMOVE IT FROM THE RIVER AT HIS/HER EXPENSE AND TO THE SATISFACTION OF THE COAST GUARD AND CITY OF CHICAGO. ALL COST RELATED TO THIS WORK SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- ALL STRUCTURAL STEEL SHALL BE AASHTO M270 GRADE XX AND SHALL BE GALVANIZED. SEE SPECIAL PROVISION FOR "HOT DIP GALVANIZING FOR STRUCTURAL STEEL".
- ALL HARDWARE, RODS, NUTS, AND PLATE WASHERS SHALL BE GALVANIZED BY THE HOT DIP PROCESS IN ACCORDANCE WITH ASTM A153. GALVANIZED HARDWARE SHALL CONFORM TO ASTM A386. STAINLESS STEEL SHALL BE OF ASTM A276, TYPE 304.
- THE TIMBER SHALL BE CREOSOTED DENSE STRUCTURAL 65 GRADE SOUTHERN YELLOW PINE CONFORMING TO THE "GRADING RULES FOR SOUTHERN PINE LUMBER OF THE SOUTHERN PINE INSPECTION BUREAU."
- 10. THE SEASONING, PREPARATION, AND TREATMENT OF TIMBER SHALL BE IN ACCORDANCE WITH THE AMERICAN WOOD PRESERVERS ASSOCIATION (AWPA) STANDARD FOR PRESERVATION TREATMENT BY PRESSURE PROCESSES FOR ALL TIMBER PRODUCTS C 1-73, AND AS SPECIFIED HEREIN.
- 11. ALL TIMBER SHALL BE THOROUGHLY AIR SEASONED BEFORE TREATMENT ACCORDING TO AWPA STANDARD M 1-72. CONDITIONING BY OIL OR STEAM SEASONING WILL NOT BE PERMITTED.
- 12. THE PRESERVATIVE SHALL BE CREOSOTE (AWPA P1/P13-91) OR CREOSOTE COAL TAR SOLUTION (AWPA P2-58, GRADE A) WHICH SHALL CONFORM TO THE REQUIREMENTS AS SET FORTH FOR EACH IN THE LATEST MANUAL OF RECOMMENDED PRACTICES OF THE AWPA. TREATMENT OF TIMBER SHALL BE IN ACCORDANCE WITH AWPA STANDARD C2-93 AND SHALL BE TREATED TO RETAIN AT LEAST TEN (10) POUNDS OF PRESERVATIVE PER CUBIC FOOT.

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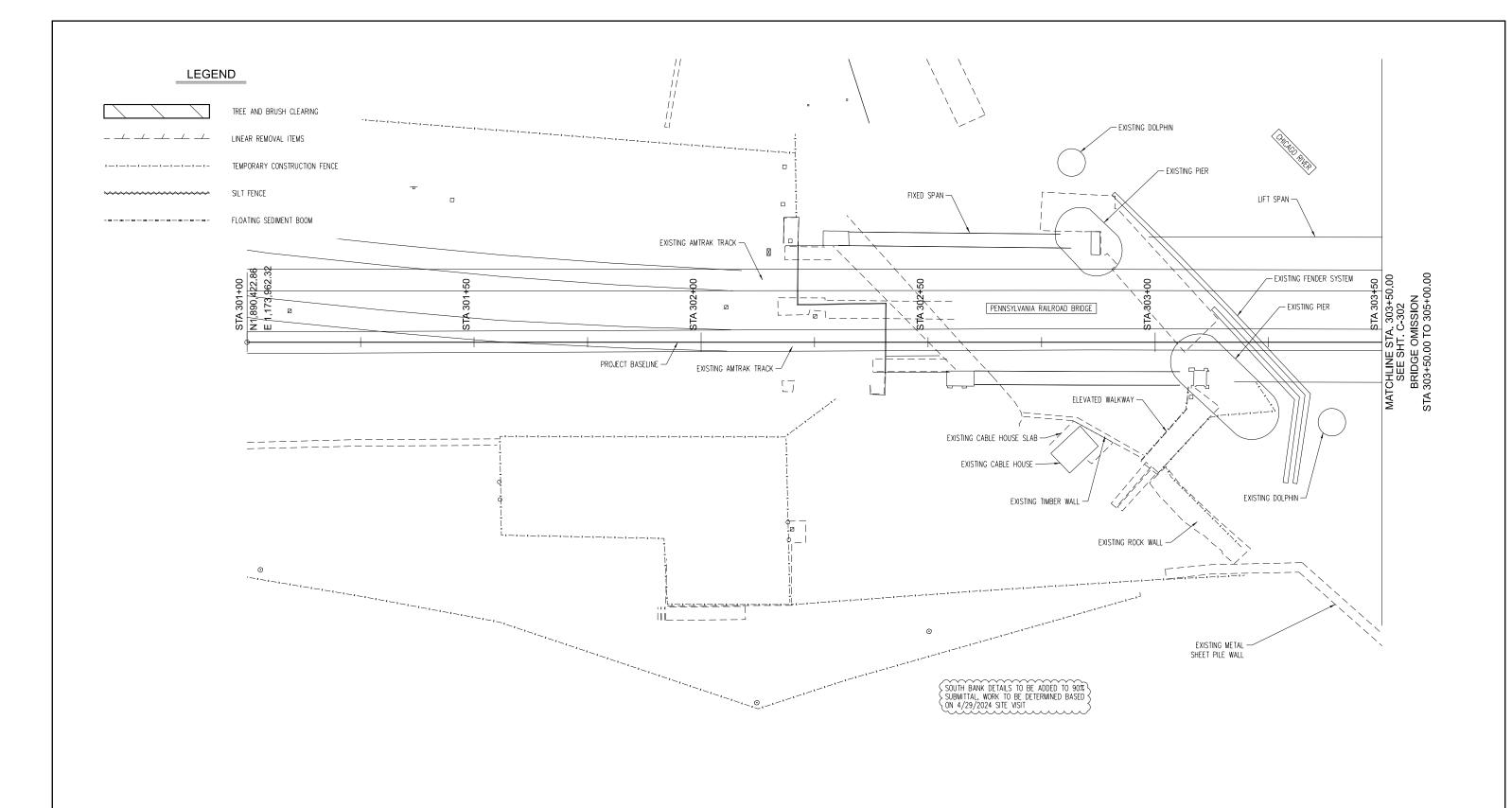
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SITE PLAN

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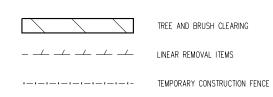
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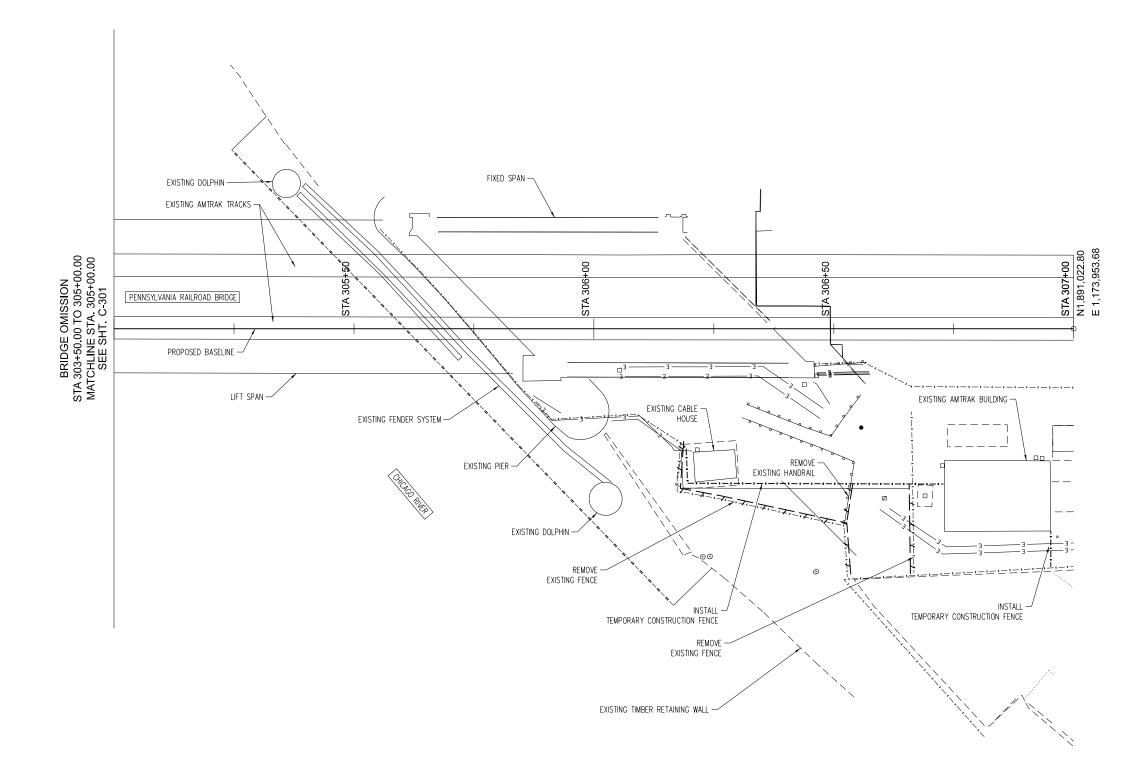
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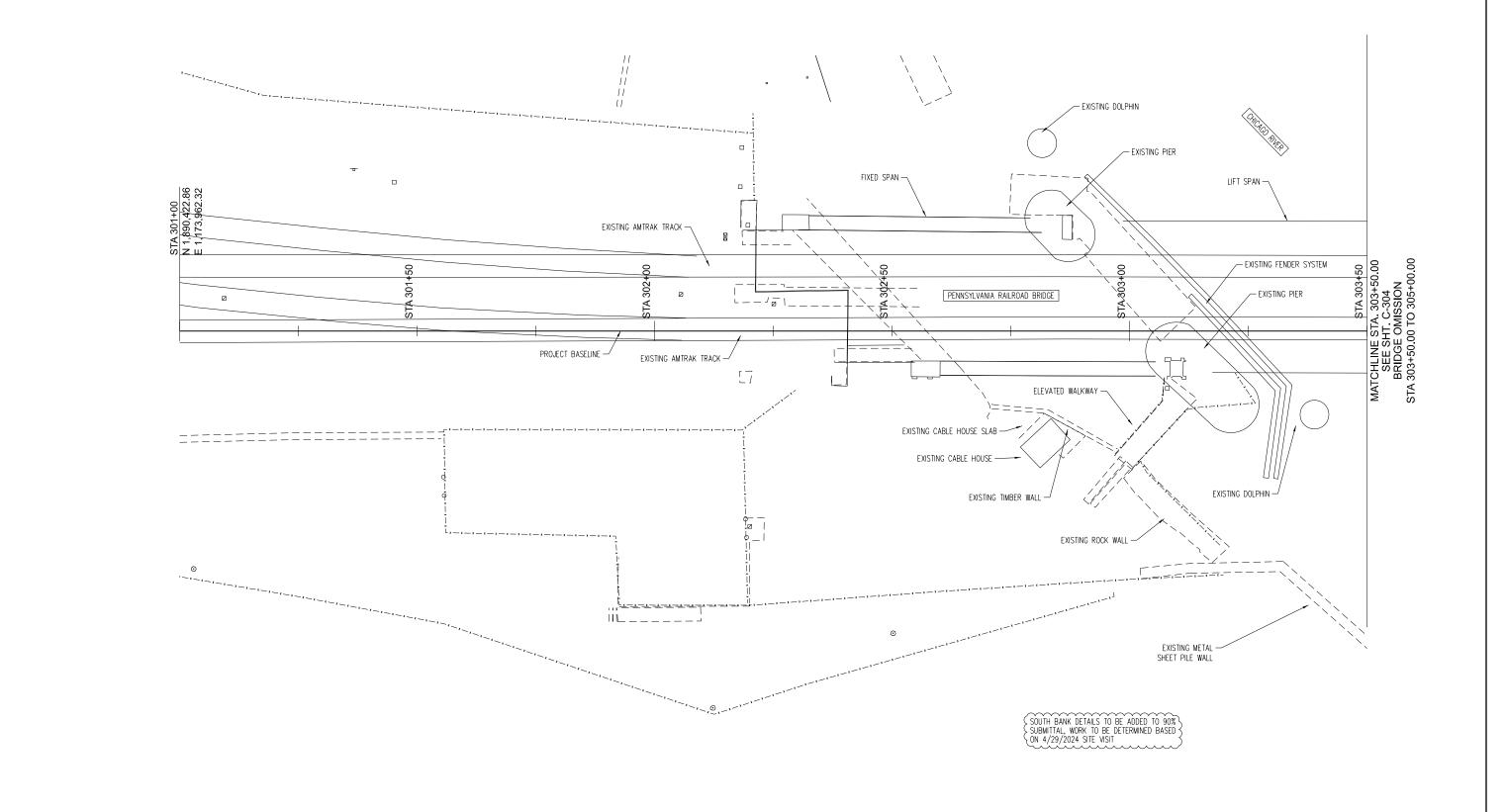
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SITE PLAN II

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SITE RESTORATION PLAN
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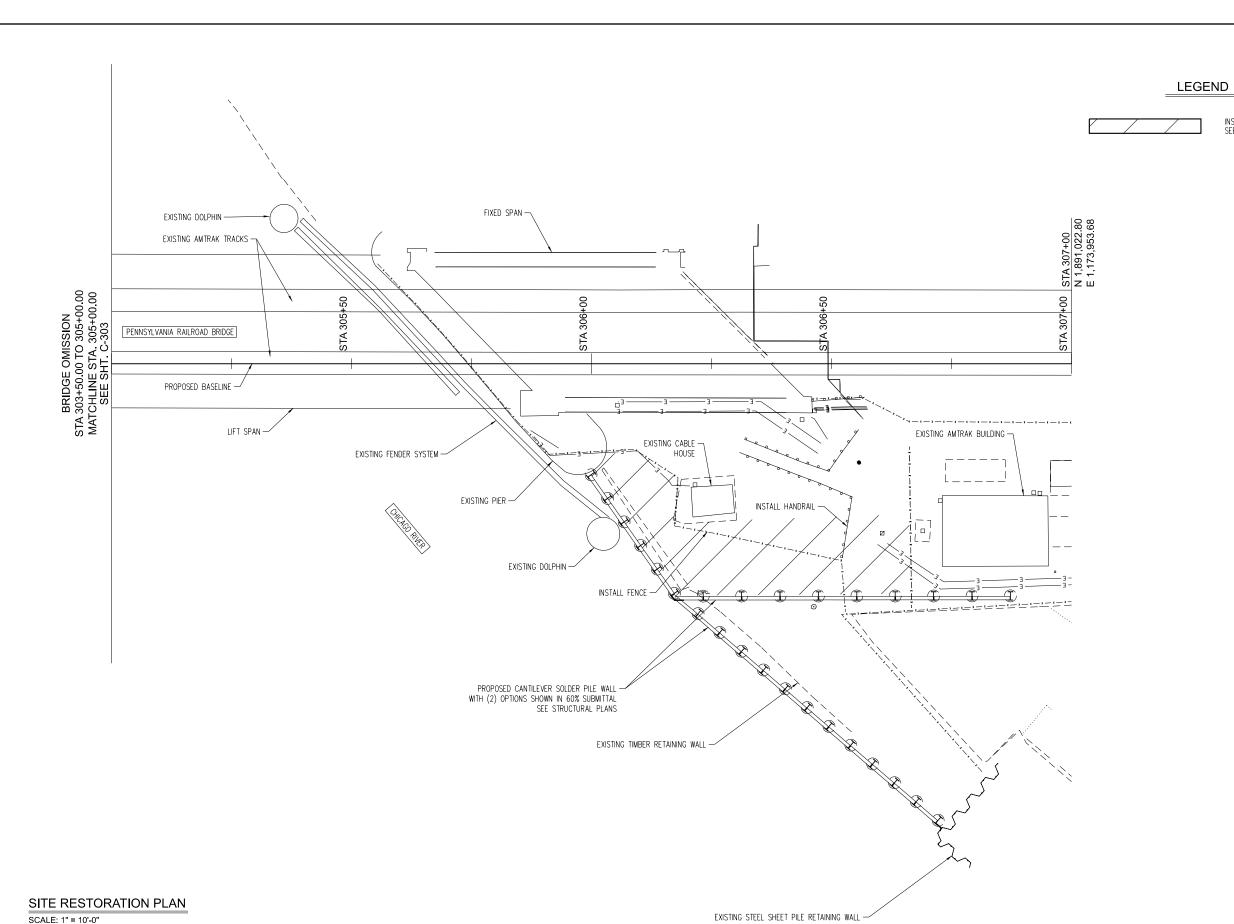
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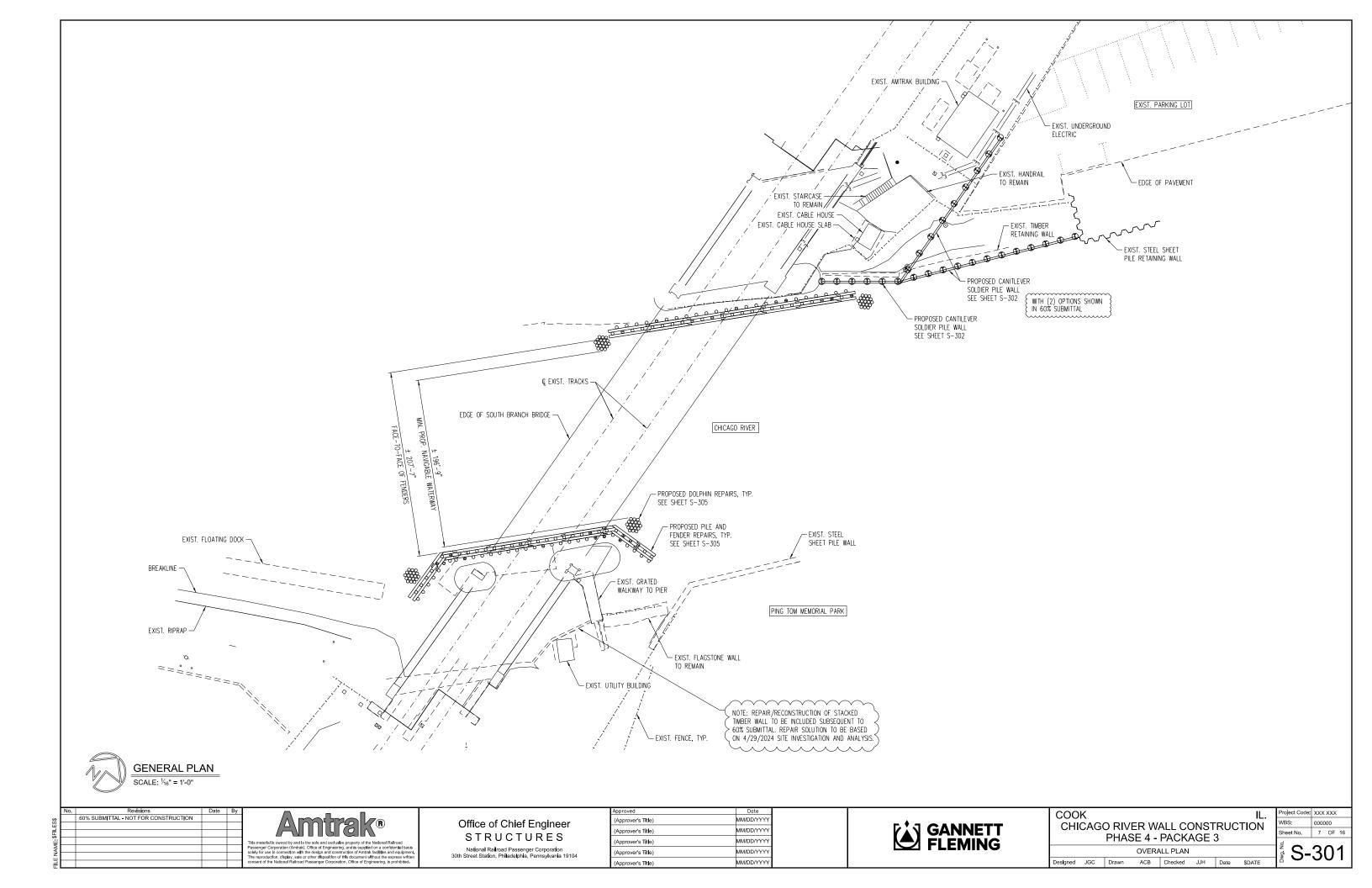


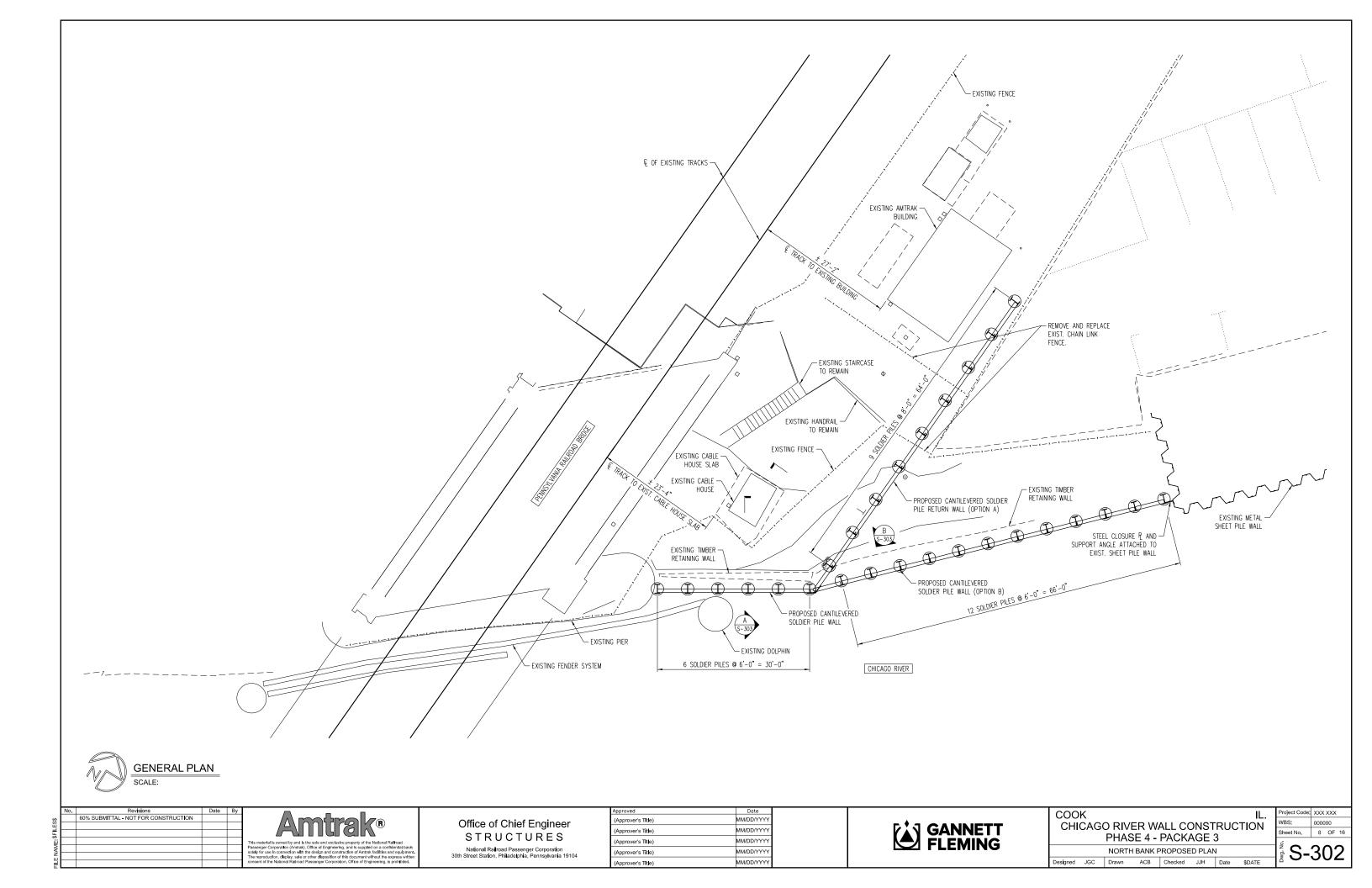
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RESTORATION PLAN III

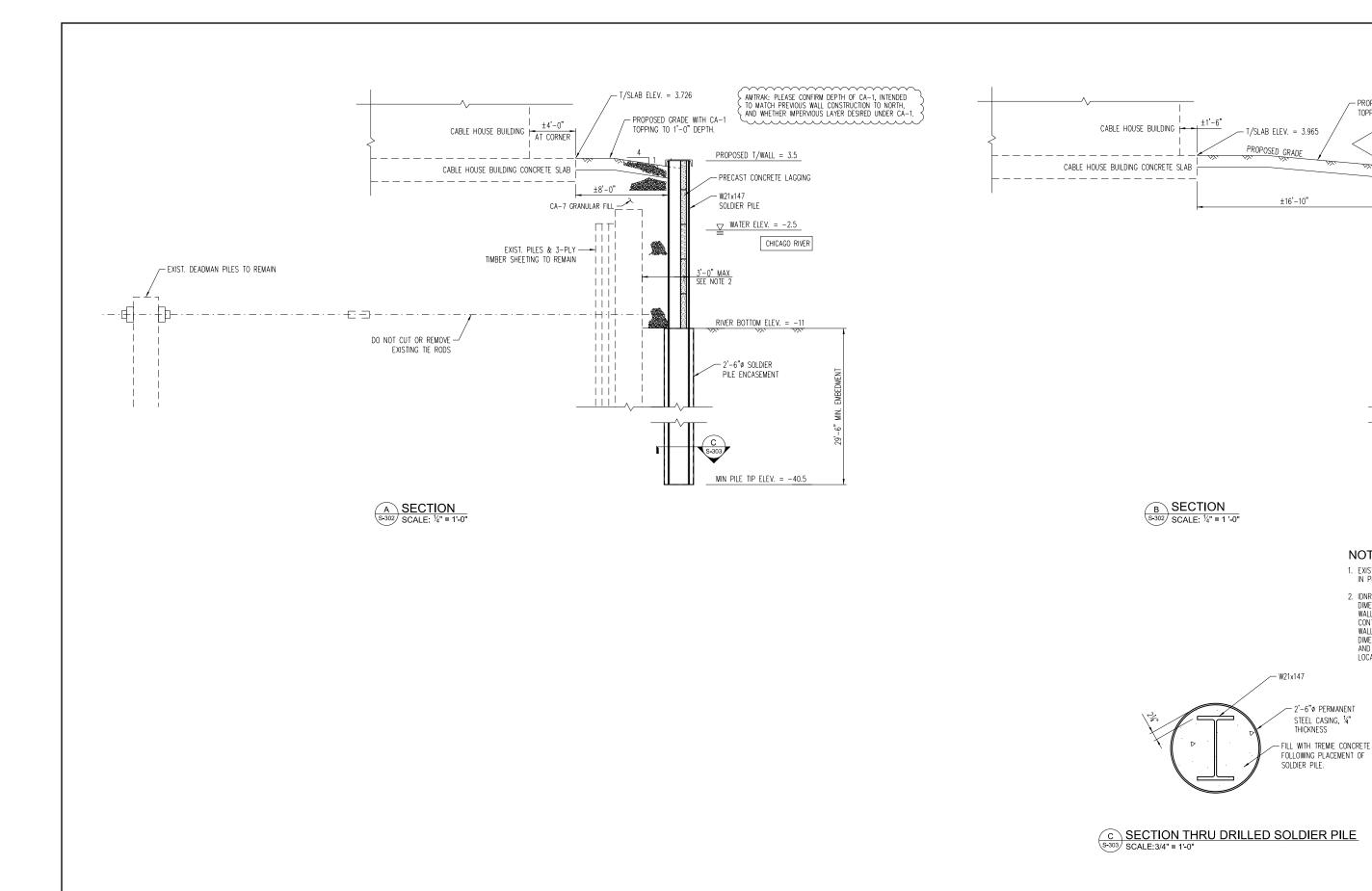
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INSTALL AGGREGATE SURFACE SEE STRUCTURAL SECTIONS FOR DETAILS

Sheet No. 6 OF 16







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PHASE 4 - PACKAGE 3
NORTH BANK SECTIONS

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- PROPOSED GRADE WITH CA-1 TOPPING TO 1'-0" DEPTH.

NOTES:

LOCATIONS.

1. EXISTING TIE RODS AND DEADMAN TO REMAIN IN PLACE. DO NOT CUT OR REMOVE TIE RODS.

2. IDNR PERMITTING ALLOWS MAXIMUM 3'-0"
DIMENSION FROM FRONT FACE OF EXISTING
WALL TO FRONT FACE OF NEW WALL.
CONTRACTOR TO LAYOUT AND INSTALL NEW
WALL ELEMENTS TO ENSURE LESS THAN 3'-0"
NEWSTRAND TO ENSURE LESS THAN 3'-0"

DIMENSION BETWEEN FACE OF EXISTING WALL AND FACE OF COMPLETED NEW WALL IN ALL

- MIN. RETAINED SOIL ELEV. = 3.00

MIN PROPOSED T/WALL = 3.50 MAX PROPOSED T/WALL = 12.50

PRECAST CONCRETE LAGGING

MIN GROUND ELEV. = -1.97 MAX GROUND ELEV. = 12.31

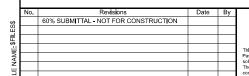
2'-6"ø SOLDIER

PILE ENCASEMENT

MIN. PILE TIP ELEV. = -40.5 MAX. PILE TIP ELEV. = -10.00

> Sheet No. 9 OF 16 S-303

REPAIR/RECONSTRUCTION OF STACKED TIMBER WALL TO BE INCLUDED SUBSEQUENT TO 60% SUBMITTAL. REPAIR SOLUTION TO BE BASED ON 4/29/2024 SITE INVESTIGATION AND ANALYSIS.



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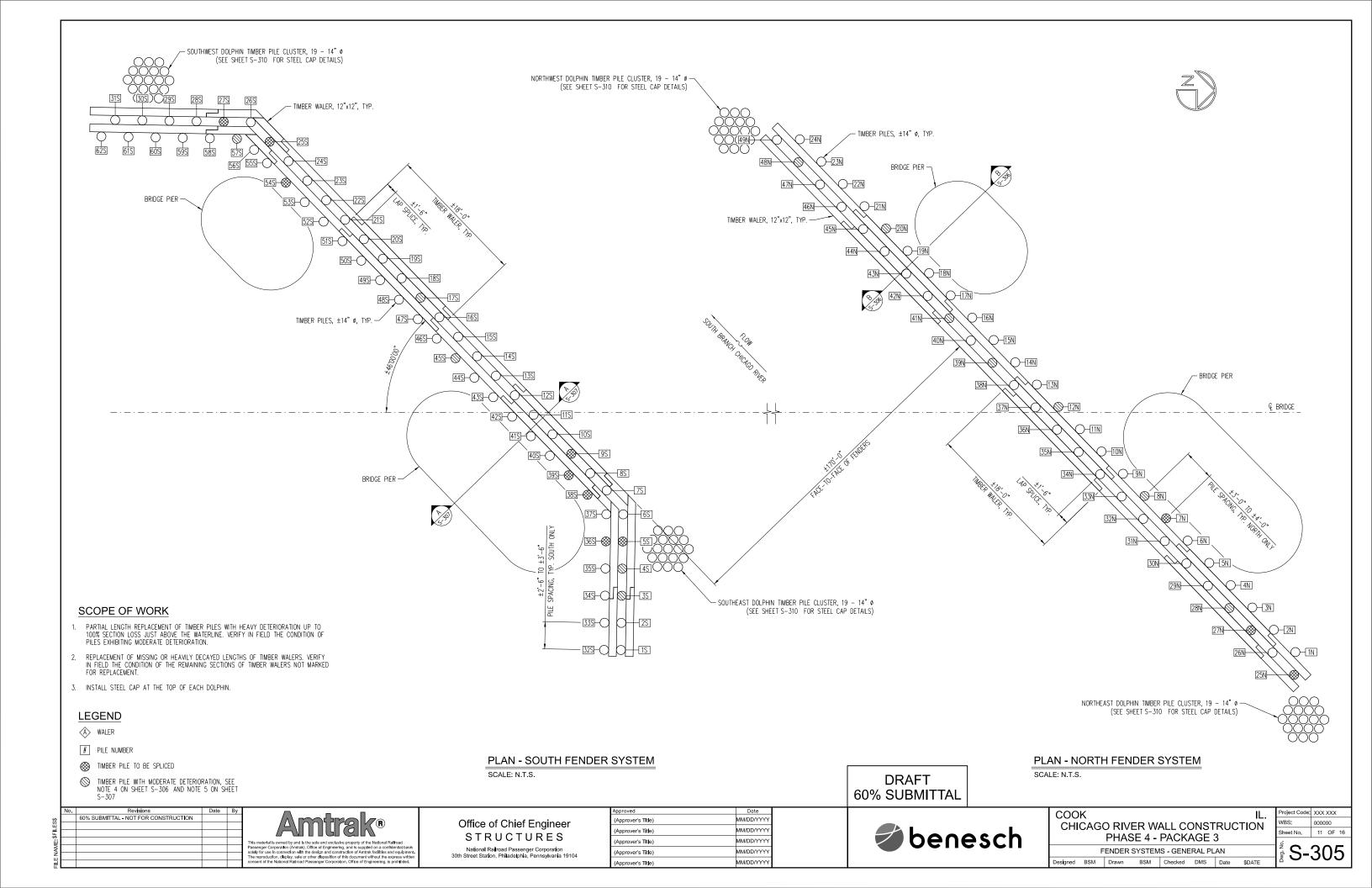


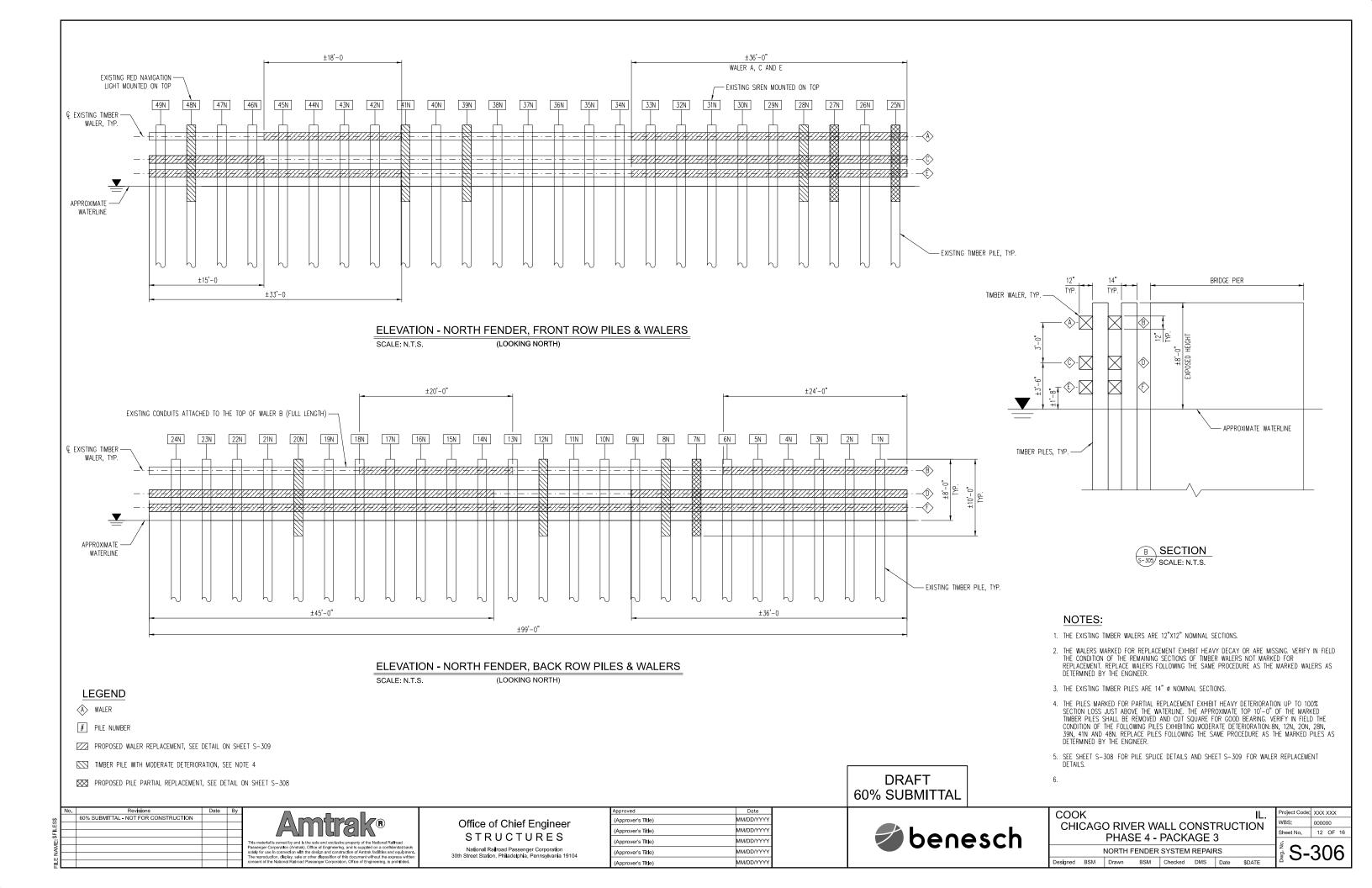
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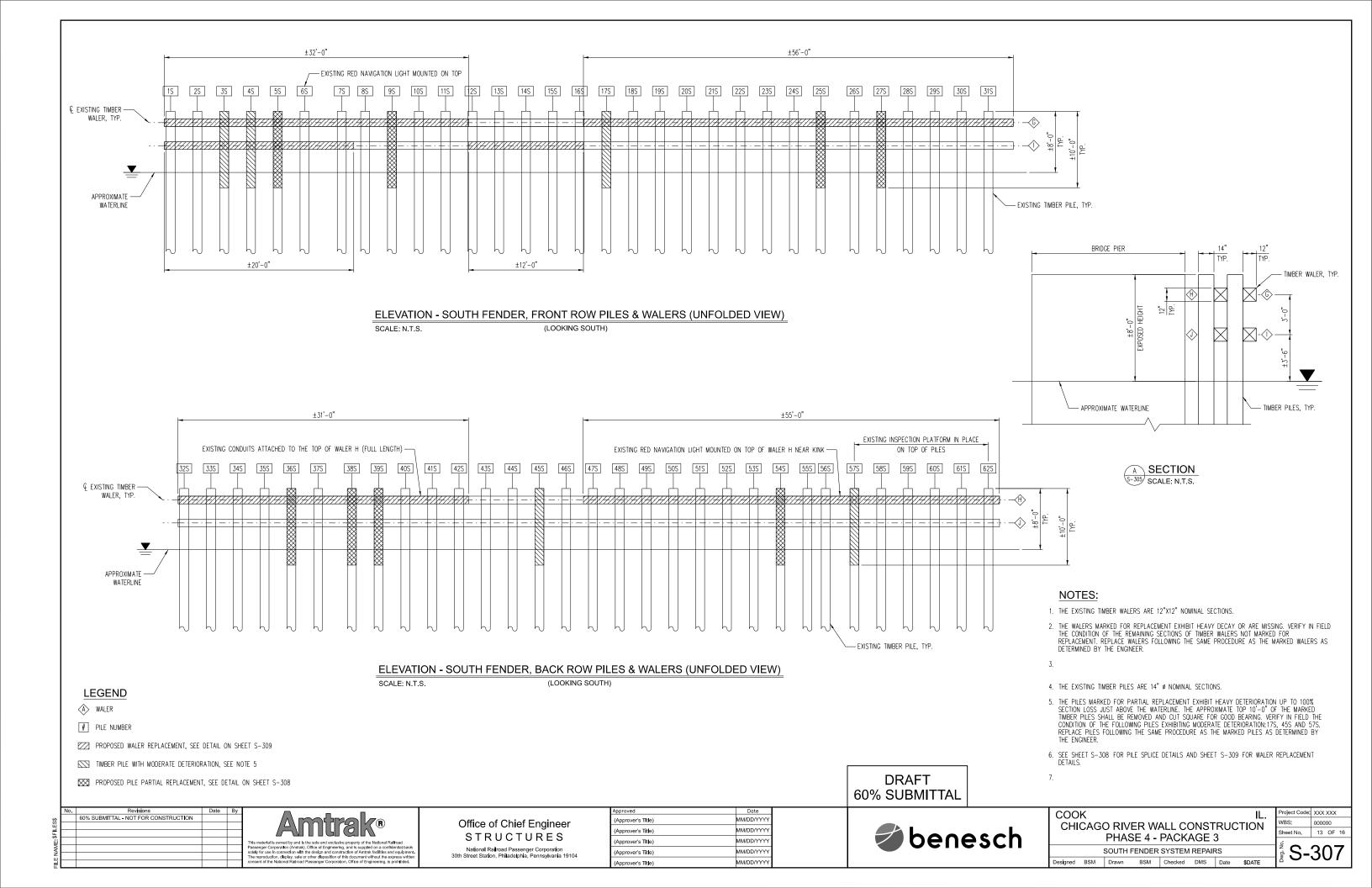
 SOUTH BANK PROPOSED PLAN

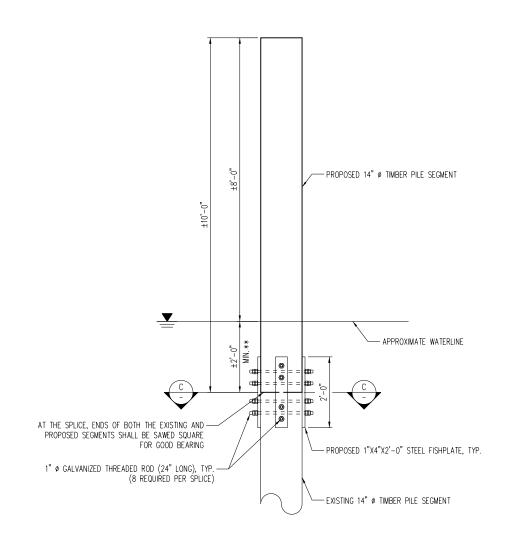
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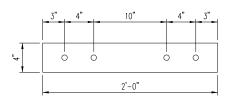




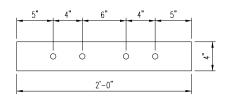


PILE SPLICING DETAIL SCALE: N.T.S.

 $\ensuremath{^{**}}\xspace$ existing timber pile to be removed until solid timber remains for a sound splice connection to be made.



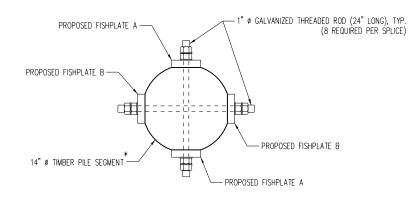
FISHPLATE A (2 REQUIRED PER SPLICE)



FISHPLATE B (2 REQUIRED PER SPLICE)

PROPOSED 1" STEEL FISHPLATES

SCALE: N.T.S.



© SECTION SCALE: N.T.S.

NOTES:

- PILE SPLICING TO BE COMPLETED THROUGH THE USE OF A DIVER TO PERFORM SUB-SURFACE WORK.
- 2. ALL GALVANIZED THREADED RODS SHALL HAVE DOUBLE HEAVY HEX NUTS WITH COMPATIBLE WASHERS AT EACH END. THE COST OF HEAVY HEX NUTS AND WASHERS TO BE INCLUDED IN THE COST OF 1" Ø GALVANIZED THREADED ROD.

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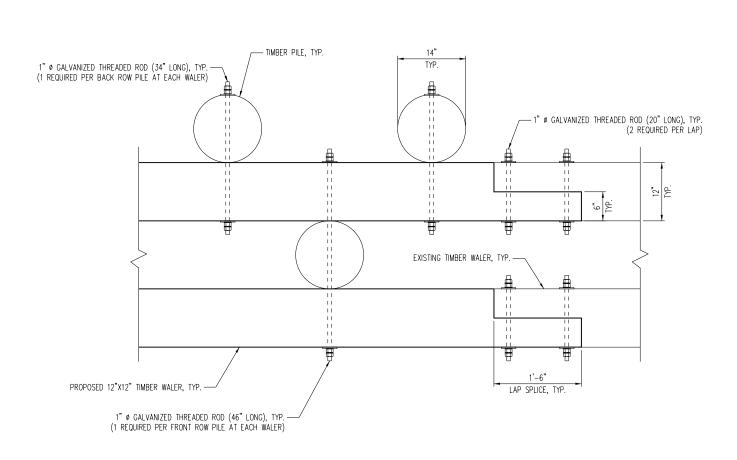
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PILE SPLICING DETAILS

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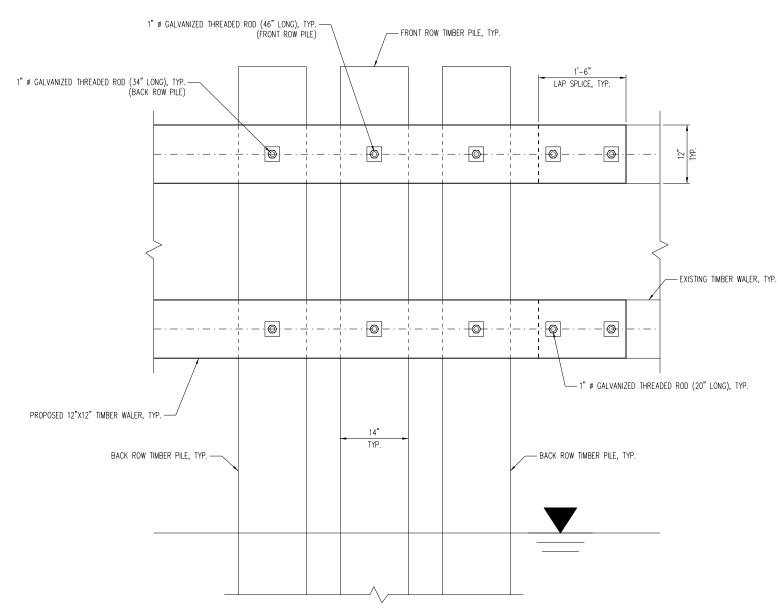
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^{*} WITHIN THE LIMITS OF THE SPLICE FACE EXISTING AND PROPOSED PILE SEGMENTS ON FOUR SIDES SO FISHPLATES FULLY BEAR ON TIMBER, THEN BOLT TIGHTLY.



PLAN - WALER LAP DETAIL & PILE FASTENING DETAIL SCALE: N.T.S.



ELEVATION - WALER LAP DETAIL & PILE FASTENING DETAIL SCALE: N.T.S.

NOTES:

1. ALL GALVANIZED THREADED RODS SHALL HAVE 4 NUTS AND 2-14"X3"X3" PLATE WASHERS. THE COST OF NUTS AND WASHERS TO BE INCLUDED IN THE COST OF 1" ø GALVANIZED THREADED ROD.

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Amtrak®

Office of Chief Engineer STRUCTURES

National Railroad Passenger Corporation
30th Street Station, Philadelphia, Pennsylvania 19104

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COOK IL.
CHICAGO RIVER WALL CONSTRUCTION
PHASE 4 - PACKAGE 3

PHASE 4 - PACKAGE 3

WALER LAP AND PILE FASTENING DETAIL

Designed BSM Drawn BSM Checked DMS Date \$DATE

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